

Exhibit 240-A. Traffic Impact Analysis Pre-Submittal Form

Project Name: Downtown Connection Center
Developer/Owner: Mountain Line
Phone Number: 928.679.8903
Email: kmorley@naipta.az.gov

Project Location

State Route (with nearest MP or Street): Business Route 40 (Milton Road)/Phoenix Ave
Local Jurisdiction: City of Flagstaff

Stage of Development (choose one)

☐ Planning/Zoning

☒ Development Plan

Brief Description of Project (land use, intensity, timeframe/phasing)

The Mountain Line Downtown Connection Center (DCC) will improve on the existing bus stops on Phoenix Ave by providing off-street bus stops, a ~20,000 sq ft office building including amenities for transit patrons and a break room for operators. The project will be developed in one phase within the next 2-3 years. No expansion of existing bus service is anticipated.

Proposed Access (number, location, restrictions)

No direct access to the state system. Access to Phoenix Avenue (local street) at two locations (one across from Mikes Pike and one approximately 200 feet east of Milton Road).

Preliminary Assumptions (provide as attachment)

- Trip Generation
- Study Horizon Years
- Trip Distribution
- Pass-By Or Internal Capture
- Future Roadway Network
- Study Area Intersections

Traffic Study Type (choose one)

- ☐ Transportation Planning Study
☒ Traffic Impact Analysis
☐ Traffic Impact Statement

Traffic Study Preparer

Firm Name: AECOM
Contact: Kordel Braley
Phone: 801.735.5554
Email: kordel.braley@aecom.com

Pre-Submittal Forms are not required for each project but are a useful tool to reduce the number of submittals/reviews and aid development timeframes. When submitted, Regional Traffic Engineering staff will review and confirm the form in a timely manner. Changes to the above information should be provided in writing. A hard copy of an approved Pre-Submittal Form shall be included in the Study appendix.

Approval by: _____ Date: _____

Preliminary Assumptions

Trip Generation

Trip generation for the new development was calculated using rates found in the *ITE Trip Generation Manual, 10th Edition*.

Land Use	Size	ITE Land Use Code	Midday			PM		
			In	Out	Total	In	Out	Total
Office	12,600	710	3	15	18	2	13	15
Other*	9,400	n/a	0	0	0	0	0	0

*Other uses include a bus driver break room and public space (restrooms, etc.) for transit patrons. These uses will not generate new trips.

No new bus service is planned for the DCC. Routing may change creating changes to volumes at isolated locations (for example Milton Road/Phoenix Ave), however, further away, traffic volumes would be unchanged.

Study Horizon Years

The TIA will analyze existing conditions (2021). The study horizon years are 2023 (opening year) and 2026 (opening year plus three years).

Trip Distribution

The trip distribution percentages for buses will be based on established routes.

New routing is not finalized and subject to change and is also dependent on whether a traffic signal is constructed at Phoenix Ave/Milton Road (see Future Roadway Network discussion below). However, the three routes are proposed to be changed as follows (Routes 4, 5, and 8):

Route	Change	Existing Movements at Milton/Phoenix	Proposed Movements at Milton/Phoenix	Peak Frequency
2 (Blue)	No Change	SBL, WBR	SBL, WBR	3/hour
3 (Green)	No Change	n/a (doesn't use Milton)	n/a (doesn't use Milton)	2/hour
4 (Gold)	Add egress to Milton	n/a (doesn't use Milton)	WBL	3/hour
5 (Orange)	Add ingress from Milton	WBR	SBL, WBR	1/hour
7 (Purple)	No Change	n/a (doesn't use Milton)	n/a (doesn't use Milton)	3/hour
8 (Teal)	Add egress to Milton	NBR	NBR, WBL	2/hour
10 (Maroon)	No Change	n/a (doesn't use Milton)	n/a (doesn't use Milton)	3-6/hour
14 (Brown)	No Change	NBR	NBR	2/hour
66 (Red)	No Change	WBR, SBL	WBR, SBL	2/hour
Mountain Express (Grey)	No Change	WBR, SBL	WBR, SBL	TBD

The trip distribution percentages for office traffic are based on previous counts and Streetlight data. The percentages are as follows:

- 49% to/from the north
- 51% to/from the south

Pass-By or Internal Capture

No pass-by or internal capture is anticipated.

Future Roadway Network

No changes to the roadway network are proposed at this time. Consistent with the recently completed Milton Road Corridor Master Plan and other locally adopted plans, Mountain Line is proposing a traffic signal at Milton/Phoenix. An analysis of this intersection is anticipated to be included in the TIA including signal warrants and proposed lane configuration.

The TIA will also include:

1. An evaluation of impacts of an at grade pedestrian crossing(s) at Phoenix on the Milton Road Corridor operations. This will be accomplished by including the pedestrian crossings in the traffic model.
2. An evaluation of the traffic operations along the Milton Road Corridor if pedestrians are not permitted to cross Milton Road at Phoenix Ave, and instead provided with a grade separated crossing.
3. An evaluation of the approach grades to Milton for the east and west legs of the intersection, assuming it is signalized.
4. The Milton Corridor CMP Phase 2 building setback will be accommodated and shown on the site plan.

The development team will follow the TIA requirements in TGP 240 and analyze mitigation measures commensurate with needs—if required—that are feasible to construct with this project.

Study Area Intersections





City of Flagstaff

Community Development Division

211 W. Aspen Ave

P: (928) 213-2618

Flagstaff, AZ 86001

F: (928) 213-2609

www.flagstaff.az.gov

Date Received (City Use)		Application for Concept/Site Plan Review				File Number (City Use)	
Site Address 216 W Phoenix Ave		Project Name Downtown Connection Center		Parcel Number 10043003B, 10043001D, 10044005A			
Property Owner(s) Bryce Doty (City of Flagstaff)		Title Real Estate	Phone 928-637-8384	Email bryce.doty@flagstaffaz.gov			
Mailing Address				City, State, Zip			
Applicant(s) Kate Morley, NAIPTA		Title Dep. Gen Manag.	Phone 928-649-8903	Email kmorley@naipta.az.gov			
Mailing Address 3773 N Kaspar Dr				City, State, Zip Flagstaff, AZ 86004			
Project Representative Jennifer Love		Title PM	Phone 602-369-2395	Email jennifer.love@aecom.com			
Mailing Address 7720 N 16th St				City, State, Zip Phoenix, AZ 85020			
Requested Review:		<input type="checkbox"/> Concept		<input checked="" type="checkbox"/> Site Plan			
Proposed Use(s) Transit Center		Existing Use Transit Center, Parking, Storage,		Subdivision, Tract & Lot Number			
Zoning District CS		Regional Plan Land Use Category urban		Flood Zone yes	Size of Site (Acres) 4.34		
Property Information:		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No Located in an existing Local/National Historic District? (Name: _____) <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No Existing structures are over 50 years old at the time of application? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No Subject property is undeveloped land?					
Surrounding Land Uses: (i.e. Res, Commercial, Ind.)		North: railroad		South: commercial		East: commercial	
						West: commercial	
Proposed Use:		Number of Units	# of Affordable Units	# of acres per use		Building Square Feet	
Duplex:							
Multi-Family:							
Commercial: Office						18,983	
Commercial: Retail							
Commercial: Restaurant							
Commercial: Service							
Commercial: Other							
Industrial:							
Institutional:							
Other:							
Please complete and submit: the "Concept Plan and Site Plan Application"; the "Application and Information Checklist"; required number of plans and information; and fees. Concept Plan fees are \$420. Site Plan fees are \$2,550 for projects less than an acre; \$1,062 plus \$1,553 per acre for sites greater than one acre. New application deadlines are every other Tuesday by 11:00 a.m. (See schedule for dates). Incomplete submittals will not be scheduled.							
Property Owner Signature		Date:		Applicant Signature		Date:	
For City Use							
Staff Assignments	Planner:	Engineer:	Public Works/Utilities:		Fire:	Stormwater	
Action:							

<p style="text-align: center;">CITY OF FLAGSTAFF</p>	<h1 style="margin: 0;">Site Plan</h1> <h2 style="margin: 0;">Development Application Checklist</h2>
---	---

Submittal Requirements:

As part of the Concept Plan Review Comments, the Planning Development Manager (PDM) assigned to your project will identify those items from this Development Application Checklist that will be required for site plan submittal. An application that does not include all required items will be rejected. By signing the application, the Applicant acknowledges that all required documents/information are included within the submittal.

If you have any questions regarding the information above or items indicated on this checklist, please contact your PDM at the contact information listed on Page 5 of this checklist.

PART I – GENERAL REQUIREMENTS

Staff Use Only		Description of Documents Required for Complete Application. No application shall be accepted without all items marked below. One complete set of all submitted documents will be required in hard copy form in addition to an electronic copy.
Req	Sub	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	I.1 Site Plan Development Application Checklist (this list)
<input checked="" type="checkbox"/>	<input type="checkbox"/>	I.2 Site Plan Application Fee Base fee \$ _____ + (Per acre fee \$ _____ x Number of acres _____) = *TOTAL: \$ _____ Traffic Impact Analysis Review Fee (if applicable) \$ _____ Development Agreement Fee (If applicable) \$ _____ = *TOTAL FEE DUE AT TIME OF APPLICATION: \$ _____ <i>*Please note, this fee calculation is good for 6 months from the date listed on Page 5 of this checklist. If submittal is to occur after expiration of this calculation, please contact the PDM listed on Page 5 of this checklist for an updated fee.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	I.3 Completed Application for Site Plan Review The application must be signed by the Applicant and the current property owner of record. If the property owner is unavailable to sign the application, a Letter of Authorization must be submitted.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	I.4 Concept Plan Review Comment Response Letter Written responses to concept plan comments or site plan revision comments.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	I.5 Project Narrative – The narrative should include the following <ul style="list-style-type: none"> Project title and date Describe project/development request Legal description of the parcel Site acreage (gross and net) Approximate building square footage, lot coverage, and FAR (non-residential projects) Number of dwelling units, types (e.g. single-family, duplex, condo, townhome, apartment, etc.) Dwelling units per acre A list of material management strategies to be employed on site (see http://flagstaff.az.gov/DocumentCenter/View/57570) Construction logistics plan (e.g. materials, parking, deliveries, impact to public right-of-way, etc.) A description of proposed stormwater management for the site Indicate whether residential units will be ownership or rental if applicable
<input checked="" type="checkbox"/>	<input type="checkbox"/>	I.6 – Electronic Submittal – An electronic copy of all submitted information (.pdf or .tif format) with individual files labeled by checklist heading type i.e. 1.3 Site Plan Application; 1.4 Comment Response Letter; 1.5 Project Narrative etc.
<input type="checkbox"/>	<input type="checkbox"/>	I.7 Board of Adjustment Variance or Waivers from Community Development Staff (if applicable)

PART II – REQUIRED PLANS & RELATED DATA

Staff Use Only		Description of Documents Required for Complete Application. No application shall be accepted without all items marked below. All hard copy plans shall be plotted no larger than 24X36 and folded into a plan set.
Req	Sub	
<input type="checkbox"/>	<input type="checkbox"/>	

City of Flagstaff Community Development Division

211 W Aspen Avenue, Flagstaff, AZ 86001 | Phone: (928) 213-2618 | Fax (928) 213-2609 | www.flagstaff.az.gov



II.1 Site Plan

Plan must be drawn to a standard engineering scale (e.g. 1:10, 1:20, but no larger than 1:60). The site plan shall include the following information:

Project Information

III Development Name

IV Site Address

V Assessor's Parcel Number (APN)

VI Parcel size (acreage, net and gross)

VII Zone District

VIII Developer's name, address, and phonenumber

IX Preparer's name, address, and phonenumber

X Residential: Proposed dwelling units per acres/permitted dwelling units per acre by zone district

XI Commercial: Proposed Floor Area Ratio (FAR)/permitted Floor Area Ratio (FAR) by zone district

XII Building(s) Information:

- Gross floor area
- Number of floors
- Proposed maximum height/permitted maximum height by zonedistrict
- Number of units and proposed commercial suite numbers and/or residential apartment numbers
- Building square footage (if multiple structures, list uses, proposed building numbers and associated square footage)
- Proposed International Building Code Occupancy Class and Construction Type
- Indicate proposed fire sprinklers if applicable
- Proposed number of off-street parking spaces/required number of off-street parking spaces
- Proposed number of off-street ADA compliant parking spaces/required number of off-street ADA spaces
- Proposed and required number of bicycle parking spaces

XIII Vicinity Map

- North arrow
- Scale
- Project Area indicated

Subject Site (An * indicates to graphically illustrate those elements on the subject site and 200-feet beyond the subject parcel's boundary)

- Scale, north arrow, date prepared, and legend
- *Parcel boundaries and dimensions
- *Contour lines at one-foot intervals (existing)
- *Zone districts
- *Building footprints (Include limits of overhangs)
- *Street improvements (e.g. curb, gutter, lane striping, sidewalks, fire hydrants, street lights, etc.)
- *Dedicated rights-of-way and streetnames
- *Points of access and driveways
- Clear view zones for points of access and driveways per AASHTO requirements
- Parking lots including circulation patterns and dimensions
- Bike racks/bike parking including manufacturer's specification sheets for bike racks/bike parking
- *Location, size, and type of existing and proposed utilities (water, sewer, reclaim water mains, water services and meters)
- *Location of Fire Department Connection, fire hydrants and fire lines
- *Pedestrian facilities
- Open space and parks
- Solid waste dumpster enclosures including height, building material, and dumpster access turning movements per ASHTO SU-30 (see material management strategies <http://flagstaff.az.gov/DocumentCenter/View/57570>)
- Easements, public and private, existing and proposed including description and dimensions
- *Walls, fences, and retaining walls including height and building material, proposed monument sign location
- Minimum required setbacks indicated from subject site's boundaries (per zone district)
- Existing and proposed structure's measured setbacks from property line to closest points of building(s) for front, sides, and rear elevations
- Natural features including forest, mature trees (greater than 8" diameter at breast height (DBH)), slopes, and drainage courses
- FEMA 100-year floodplain elevations, flood zone (FEMA) and type (urban or rural), floodplain limits, floodway limits (if applicable) and rock outcroppings (see Section II.9 below)
- Total existing and total proposed onsite impervious surface area calculations (roof area, pavement, sidewalk, etc.)
- Total Runoff Capture Volume (ROCV) required (the volume of water from all new impervious surfaces from a 1" rain)
- Location of existing (if any) and proposed Detention Facilities and Low Impact Development (LID) Integrated Management Practices (IMPs) Identify those IMP's to be utilized as ROCV facilities.
- Approximate area and volume of each of the IMPs to be utilized as ROCV facilities (total volume should equal or exceed the ROCV)
- Estimated finished floor elevations for allbuildings

City of Flagstaff Community Development Division

211 W Aspen Avenue, Flagstaff, AZ 86001 | Phone: (928) 213-2618 | Fax (928) 213-2609 | www.flagstaff.az.gov

<input checked="" type="checkbox"/>	<input type="checkbox"/>	II.2 Building Floor Plans Floor plans must be drawn to a standard architectural scale (1/4"=1' is preferred). The following information must be included/identified on the floor plans: <ul style="list-style-type: none"> • Interior room divisions • Proposed commercial suite numbers and/or residential apartment numbers • General function or use of each room • Interior wall height • Door and window openings • Plumbing fixtures • Dimensions for each room, including total area • Identify internal solid waste collection areas and systems for units and workspaces
<input checked="" type="checkbox"/>	<input type="checkbox"/>	II.3 Building Elevations Elevations must be drawn for all sides of the building to a standard architectural scale (1/4"=1' is preferred). The following information must be included on the elevation drawings: <ul style="list-style-type: none"> • Color elevations for all sides of all structures as they will appear upon completion, including finished grade • Building materials and finishes for all exterior surfaces, including roofs • Window Detail • Color and LRV (Light Reflectance Value) of all exterior surfaces, including roofs • Building height (graphic and written) • Specification sheets depicting finish and color for all exterior building materials and surfaces, including roofs • Proposed building mounted signage location
<input checked="" type="checkbox"/>	<input type="checkbox"/>	II.4 Landscape Plan Landscape plans shall be drawn to the same engineering scale as the site plan. For simple site plans, the Planning Development Manager may waive the requirement for a separate landscape plan and allow for the inclusion of the landscape requirements on the site plan. All landscape plans must include or show the following information, in accordance with Section 10-50.60.030 of the Zoning Code: <ul style="list-style-type: none"> • The location, size, and species of all proposed plantings, scaled to represent the size of the landscaping at maturity • Existing trees that are being used to offset landscape requirements • Groundcover for all landscaped or disturbed area • Landscaping calculations per the Zoning Code • Irrigation systems including lines and Hydrozones (when required) • Hose bibs (when used in lieu of an automatic irrigation system) • Plant species, quantity, and size table • Preliminary detention facilities • Low Impact Development (LID) Integrated Management Practices (IMPs) systems and their locations on the site.
<input type="checkbox"/>	<input type="checkbox"/>	II.5 Natural Resource Survey and Protection Plan A natural resource survey and protection plan must show all natural resources on the site before and after development (refer to Section 10-50.90.080 of the Zoning Code for applicability). When not required, a written statement stating such must be included in the project narrative, on the site plan, and on the landscape plan. The following items must be included on the natural resource survey and protection plan: <ul style="list-style-type: none"> • Forest canopy • Moderate slopes (17 to 24.99 percent) • Steep slopes (25 to 34.99 percent) • Slopes equal to or greater than 35 percent • Floodplain (rural and urban) • Calculations demonstrating that the minimum resource protection standards are met • Other site features
<input checked="" type="checkbox"/>	<input type="checkbox"/>	II.6 Grading and Drainage Plan Grading and drainage plans must include all information specified by staff as well as the following: <ul style="list-style-type: none"> • Contour lines at one-foot intervals (existing) • Estimated limits of cut and fill (site only) Identify existing offsite flows (entering the site), drainage pathways, and discharge points (exiting the site) • FEMA 100-year floodplain elevations, floodplain limits, and floodway limits (if applicable) • Identify low flow channel. • Location(s) of LID Integrated Management Practices (IMPs) Identify those IMP's to be utilized as ROCV facilities • Existing and proposed detention facilities • Identify proposed stormwater conveyance features (i.e. culvert, drainage ditches, swales, etc.) Include flow arrows, spot grades or contours as necessary to show onsite flow routing.

City of Flagstaff Community Development Division

211 W Aspen Avenue, Flagstaff, AZ 86001 | Phone: (928) 213-2618 | Fax (928) 213-2609 | www.flagstaff.az.gov

PART III – REQUIRED REPORTS, STUDIES, ANALYSIS, & RELATED DATA

Staff Use Only		Description of Documents Required for Complete Application. No application shall be accepted without all items marked below
Req	Sub	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	III.1 Cultural Resource Study All cultural resource studies must include the following information: <ul style="list-style-type: none"> Introductory information (identification of the development, property owners, clients, study preparers, contents, and index) A description of the study area and context and a description of the study boundaries and how these were determined A description of existing conditions A description of proposed work A summary of research results; review of literature and records (AZSITE, ASLD, Government Land Office Maps, and Sanborn Maps, land use records and so forth) A detailed description of the site history A complete description and evaluation of the significance and integrity of actual and potential cultural resources An evaluation of potential impacts of proposed work on actual or potential cultural resources including any indirect or residual impacts Specific recommendations for mitigation of major impacts on actual or potential cultural resources When appropriate, specific recommendations for additional research and documentation
<input checked="" type="checkbox"/>	<input type="checkbox"/>	III.2 Preliminary Drainage Documentation (Report, Letter, or Statement) Please contact stormwater staff for submittal requirement
<input checked="" type="checkbox"/>	<input type="checkbox"/>	III.3 Drainage Impact Analysis Please contact stormwater staff for submittal requirements
<input checked="" type="checkbox"/>	<input type="checkbox"/>	III.4 Water and Sewer Impact Analysis Please contact water services staff for submittal requirements and fees
<input checked="" type="checkbox"/>	<input type="checkbox"/>	III.5 Traffic Impact Analysis Please contact traffic staff for submittal requirements (if required fee must be paid at time of submittal) <i>Currently coordinating with ADOT and City Traffic on TIA.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	III.6 Title Report – No older than 30 days from the submittal date complete Schedule A and Schedule B
<input checked="" type="checkbox"/>	<input type="checkbox"/>	III.7 Alta Survey or easement and encumbrance map delineating all easements and encumbrances from the title report in graphic form
<input type="checkbox"/>	<input type="checkbox"/>	III.8 Phase I Environmental Study (only if dedications will be made)
<input checked="" type="checkbox"/>	<input type="checkbox"/>	III.9 Preliminary Logistics Report
<input type="checkbox"/>	<input type="checkbox"/>	III.10 Notice of Right to appeal exactions owner certification

PART IV – OTHER SUBMITTAL REQUIREMENTS

Staff Use Only		Description of Documents Required for Complete Application. No application shall be accepted without all items marked below
Req	Sub	
<input type="checkbox"/>	<input type="checkbox"/>	IV.1 Other Requirements Please provide the following:

City of Flagstaff Community Development Division

211 W Aspen Avenue, Flagstaff, AZ 86001 | Phone: (928) 213-2618 | Fax (928) 213-2609 | www.flagstaff.az.gov

Site Plan Application Checklist

Revision Date: 04/2020

PART V – PLANNING DEVELOPMENT MANAGER

No application shall be accepted without a Planning Development Manager signature below.

V.1 Planning Development Manager Contact Information

If you have any questions regarding this application checklist, please contact your Planning Development Manager (PDM). If you did not receive a completed copy of this Development Application Checklist as part of your Concept Plan Review Comments, please contact the PDM assigned to your Concept Plan application.

PDM Name (print): _____ Phone: _____

PDM E-mail: _____ Date: _____

PDM Signature: _____

City of Flagstaff Community Development Division

211 W Aspen Avenue, Flagstaff, AZ 86001 | Phone: (928) 213-2618 | Fax (928) 213-2609 | www.flagstaff.az.gov

Appeals of Dedication, Exactions or Zoning Regulations

Right to Appeal

An applicant may appeal the following actions to an administrative hearing officer:

1. A dedication or exaction required as a discretionary administrative, but not legislative, condition of granting approval for the use, improvement or development of real property.
2. The adoption of amendment of a zoning regulation that creates a taking of property in violation of A.R.S § 9-500.13.

Appeal Procedures

The applicant who intends to exercise their right to contest the requirement of a dedication or exaction shall file a written request for appeal to the Planning Director, who will transmit the request for appeal to the designated hearing officer.

- The appeal requires shall be filed within 30 days after notice is given of the final determination of the development or exaction requirement.
- The request for appeal may in the form of a letter or other written communication but shall give reasonable notice that the applicant requests an appeal of dedication or exaction requirement and of the particular dedication or exaction being appealed.
- After receipt of an appeal, the hearing officer shall schedule a time for the appeal to be heard not later than 30 days after receipt, unless the applicant consents to an extension of time. The applicant shall be given at least 10 days; notice of the time when the appeal will be heard unless the applicant agrees to a shorter time period.
- In all proceedings the City has the burden of establishing an essential nexus between the dedication or exaction and a legitimate government interest and that the proposed dedication or exaction is roughly proportional to the impact of the proposed use, improvement, or development. If more than a single parcel is involved this requirement applies to the entire property.
- The hearing officer shall hear such testimony and consider such evidence as is relevant to the determination of such issues. The hearing officer shall not be bound by technical rules of evidence or procedures in conducting the hearing.
- The hearing officer shall decide the appeal within five working days after the appeal is heard. If the City does not meet its burden the hearing officer shall either:
 - Modify or delete the requirement of the dedication or exaction appealed in compliance; or
 - In the case of a zoning regulation appealed, the hearing officer shall transmit a recommendation to the Council. The Council may accept, modify or deny the recommendation of the hearing officer.
- If the hearing officer modifies or affirms the requirement of the dedication or exaction, an applicant aggrieved by the decision of the hearing officer may appeal the decision to the Superior Court, at any time within 30 days after the hearing officer has rendered a decision, by following the procedures set forth in the Arizona Revised Statutes.

Please be aware that City Staff cannot give you legal advice. You may wish, but are not required, to hire an attorney to represent you in an appeal.

Owner Certification Acknowledging Receipt
Of
Notice of Right to Appeal Exactions and Dedications

I hereby certify that I am the owner of property located at:

(address where development approval or city required improvements and dedications are required)

and hereby certify that I have received a notice that explains my right to appeal all exactions and/or dedications required by the City of Flagstaff as part of my property development on the parcel listed in the above address.

Signature of Property Owner

Date

**MINOR APPROVAL PROCESS
(NO IMPACT ANALYSIS REQUIRED)**

APPLICATION TYPES:

- | | |
|---------------------------|---------------------------------|
| 1. Conditional Use Permit | 4. Preliminary Plat |
| 2. Zoning Map Amendment | 5. Plat Revision |
| 3. Site Plan | 6. Modified Subdivision Process |

TIMELINE

ADMINISTRATIVE COMPLETENESS REVIEW	26 days
------------------------------------	---------

SUBSTANTIVE REVIEW	22 days
--------------------	---------

OVERALL TIME FRAME	48 days ¹
--------------------	----------------------

Notes: ¹This time period includes a second review after return of comments on the first review. Total time is less if only one review is needed for a particular approval

COMPLIANCE REVIEW TIMEFRAMES

**MAJOR APPROVAL PROCESS
(IMPACT ANALYSIS REQUIRED)**

APPLICATION TYPES:

- | | |
|---------------------------|---------------------------------|
| 1. Conditional Use Permit | 6. Final Plat |
| 2. Zoning Map Amendment | 7. Modified Subdivision Process |
| 3. Site Plan | 8. Minor Plan Amendment |
| 4. Preliminary Plat | 9. Development Agreement |
| 5. Plat Revision | 10. Annexation |

TIMELINE

ADMINISTRATIVE COMPLETENESS REVIEW	26 days
------------------------------------	---------

SUBSTANTIVE REVIEW	52 days
--------------------	---------

OVERALL TIME FRAME	78 days ¹
--------------------	----------------------

Notes: ¹This time period includes a second review after return of comments on the first review. Total time is less if only one review is needed for a particular approval



City of Flagstaff Community Development Concept Plan Review Comments

Project Name: Mountain Line Downtown Connection Center
Project No.: PZ-20-00094-01
Project Address: 216 W Phoenix Avenue

Date: October 26, 2021

Application Type: Concept Plan

Project Manager: Tiffany Antol
E-mail: tantol@flagstaffaz.gov
Phone: 928-213-2605

Status of Review: **Complete**
Responses in red.

CONDITIONS/COMMENTS:

Current Planning: Tiffany Antol
Approved with Conditions, 10/26/21

Conditions of Approval

1. The parking demand study proposes to reduce the required parking for the new DCC building more than 30% which will require that the Parking Demand Study be approved by City Council. In addition, it is proposed that the required parking based on the study be provided with the existing city inventory of parking. Please provide written approval from the property owner allowing the use of these parking spaces in both Phase 1 and 2 of the project. **City Manager can sign.**
2. A Conditional Use Permit is required for the proposed project. A CUP application shall be submitted and deemed complete at least 30 days before scheduling a public hearing. **Noted.**
3. A site plan for each phase of development shall be submitted and approved prior to the submission of construction review documents (civil or building). **Noted.**

Development Engineering: David Millis
Approved with Conditions 08/30/2021

Conditions of Concept Plan Approval

The Modification to the Engineering Design Standards for the back-in on-street parking has been conditionally approved subject to not otherwise being impacted by the outcome of the TIA. **Design team coordinating with traffic on turn lane at intersection and layout of back in parking. Modification to Engineering Design Standards will be prepared once approach to Milton/Phoenix layout is confirmed.**

Conditions of Future Submittals

1. Project meets thresholds for Public Improvements under 10-30.50.
 - a. Provide streetlights along frontage per 13-12-003 **This will be a requirement included at Final Design.**
 - a. Phoenix Ave is classified as a Commercial Local street however the high bus traffic results in daily ESALs far in excess of a typical Commercial Local. Phoenix Ave. pavement section to be improved as part of this project per geotechnical analysis and design reflective of current and planned bus traffic ESALs. **This will be a requirement included at Final Design.**



City of Flagstaff Community Development Concept Plan Review Comments

- b. Provide 5' parkway with street trees and 5' sidewalk along Phoenix and 6' sidewalk with 5' parkway along Milton frontage. If there are areas where this is not possible/practical based on the unique considerations of the DCC and/or site conditions provide written request for modification to Engineering Design Standards. **This will be a requirement included at Final Design.**
- c. Remove hydrant at Phoenix and Milton and install new hydrant behind sidewalk and a minimum of 10' from intersection curb return and in conformance with 13-09-006-0006 **Revised**
- d. Fire line from main to fire sprinkler backflow preventer may not exceed 100 feet. **This will be a requirement included at Final Design.**

Building & Safety: Victoria St. Clair

Approved with Conditions 05/20/2021

Conditions of Concept Plan Approval

1. A Commercial Building Permit is required for demolition of the existing building. A Commercial Building Permit is required for the construction of the proposed building. The services of an Arizona Registered Design Professional is required. For the demolition permit use the Commercial Building Permit Application and complete the Demolition Questionnaire. The Commercial Building Permit Application, Demolition Questionnaire, standard building permit submittal requirements and list of adopted/amended building codes can be obtained from City of Flagstaff Building Safety website: <https://www.flagstaff.az.gov/494/Building-Safety>. **Noted.**

General Comments

1. The Building Safety Program reserves the right to make additional comments during actual plan submittal and review of building permit applications. **Noted.**

Fire Prevention: Christopher Jack

Approved 05/17/2021

No Comments

Public Works: Scott Overton

Approved 05/19/2021

No Comments

Stormwater: Douglas Slover

Approved 05/19/2021

General Comments

1. none.

Future Submittal Requirements

1. Provide preliminary drainage report with Site Plan submittal. **Provided at Site Plan submittal.**

Water Services: Jim Davis

Approved 05/17/2021

General Comments:

1. Per City Code SECTION 7-03-001-0015 Cross Connection Control, the developer shall install a City Approved Containment Back Flow Prevention Assembly with a wye-strainer upstream of the assembly and at least one union within 12 inches of the assembly on the domestic and potable irrigation service connections. Proposed installation shall be at the water meter, in a hot box with electricity for freeze protection. Location shall be reviewed and approved by the Industrial Waste Division. Please contact the Water Services Industrial Waste Section at (928) 213-2117 to determine the type of Back Flow Assembly to be installed. **This will be a**



City of Flagstaff Community Development Concept Plan Review Comments

requirement included at 30% Design.

2. Per City Code SECTION 7-03-001-0015 Cross Connection Control, the developer shall install a City Approved Double Check Back Flow Prevention Assembly on the fire service riser connection inside the fire riser room. If the fire sprinkler system contain any chemicals (ethylene glycol), an approved Reduced Pressure Zone Back Flow Assembly with a floor drain capable of a full port discharge will be required. Location shall be reviewed and approved by the Industrial Waste Division. Please contact the Water Services Industrial Waste Section at (928) 213-2117 to determine the type of Back Flow Assembly to be installed. **This will be a requirement included at 30% Design.**
3. Per City Code SECTION 7-03-001-0011, water meters and service lines shall be sized per AWWA Manual M22 to meet the domestic and/ or fire sprinkler pressure and flow demands. Sizing will be submitted to the City Utilities Department by the developer or owner for City review. Caution: this information is needed early in the design process so that corrections are not made after the service lines are installed. Water meter, service tap sizes, and service line length & size shall be shown on the civil and/or construction plans. Private fire lines to not require a fire service meter and meter vault. **This will be a requirement included at 30% Design.**

Parks: Amy Hagin

Approved with Conditions 05/20/2021

1. Please indicate the FUTS location based on Rio de Flag construction drawings with a note that constructed by others. **Revised.**

Sustainability: Jenny Niemann

Approved 05/20/2021

1. The applicant is strongly encouraged to construct electric-only buildings. This would enable the site to avoid adding or reconfiguring natural gas infrastructure. Electric furnaces, heat pumps and water heating technologies have improved dramatically, making electric-only buildings feasible and sometimes even preferred. Electric-only buildings allow the development to eventually become a zero-emissions development, as the electric grid becomes a zero-emissions grid. (APS has committed to net zero emissions by 2050). Installing natural gas infrastructure locks this development into consuming fossil fuels until a costly retrofit is made. Forgoing natural gas also has health benefits, resulting in healthier indoor air. Gas appliances and heating devices generate a variety of air pollutants that have been linked to health risks. **Noted.**
2. The applicant is encouraged to consider the installation of solar panels (also known as photovoltaic, or PV panels), on available roof space. Larger roofs such as this building are some of Flagstaff's best opportunities for the installation of solar infrastructure within city limits. This site may have solar potential, depending on the building to the south. Solar can benefit developments in several ways, including resilience benefits, marketing benefits, and utility cost savings. Solar is now the cheapest energy source in history, and the developer is encouraged to consider an investment in on-site energy production. Tax incentives may be available. APS currently has programming that may be available to the applicant to support solar installation and provide financial benefits. COF sustainability staff is happy to support the applicant in this process if desired -- feel free to contact us. **Noted.**

Heritage Preservation: Mark Reavis

Approved with Conditions 05/17/2021

1. HPO concurs with site development concept and building location. Building orientation appears to be relevant to both Phoenix and Milton. Approved condition is with the orientation concept being dependent upon a strong facade presence facing Milton and compatibility with Phoenix Ave. Additional facade design review will be applicable. **Noted.**



City of Flagstaff Community Development Concept Plan Review Comments

Multimodal: Martin Ince

Approved with Conditions 05/24/2021

1. Show the conceptual future alignment for the FUTS/cycletrack connection to the north to the planned underpass at BNSF. Check grades for the underpass against the Rio de Flag plans; the underpass should be close to matching grades on either side, so the trail can remain fairly flat. **Noted.**
2. Will the strip between Phoenix Ave and the first bus bay be all concrete, except for the trees in grates? **Yes, pedestrianway and passenger loading/unloading.**
3. Suggest additional crosswalks at each end between the first and second bus islands. Without them some patrons will be required to cross the driveway and internal aisles twice and have added out-of-direction travel versus crossing an internal aisle once and having a more direct route. **Noted, will examine and include additional pedestrian access to bus bays.**
4. The easterly leg of the crosswalk on Phoenix at Mikes Pikes should be closer to perpendicular. The bikeway facility crossing here is intended to continue south along the east side of Mike Pike as a two-way cycletrack. As a result the crossing will ultimately be designed as one leg of a protected intersection. **Revised.**
5. Along the planned FUTS extension east to Beaver St, the existing re-purposed railroad bridge will be removed but not replaced as part of the Rio de Flag project. Some planning at this point is needed to account for how this connection will be made in the future. There is some benefit to designing the connection for bicyclists as well as pedestrians. **Will tie into existing FUTS, exact alignment TBD.**
6. It is important that the new traffic signal at Phoenix and Milton include pedestrian crossings of Milton. **Noted, this is included in the TIA underway.**
7. Include facilities for bicycle storage on the site, both short-term parking for employees and longer-term storage for patrons. An indoor room, enclosed or covered parking, and bike lockers are all options. A bike station concept should be considered as part of programming for the building. **Bike parking is shown for Phase 1 and additional bike storage options will be identified in Phase 2.**
8. Would encourage additional discussion with the City on how to make this section a transit-pedestrian-bike-emphasis street. **Noted.**

Traffic: Reid Miller

Approved with Conditions 08/27/2021

1. A Traffic Impact Analysis (TIA) is under review. The conclusions and recommendations of the approved TIA shall be applied to the Site Plan approval. **TIA currently under review.**

NAIPTA MOUNTAIN LINE

DOWNTOWN CONNECTION CENTER (DCC)

216 W. PHOENIX AVENUE, FLAGSTAFF, AZ 86001

APN: 100-43-003B

SITE PLAN SUBMITTAL - PHASE 1

NOVEMBER 9, 2021

PROPERTY OWNER

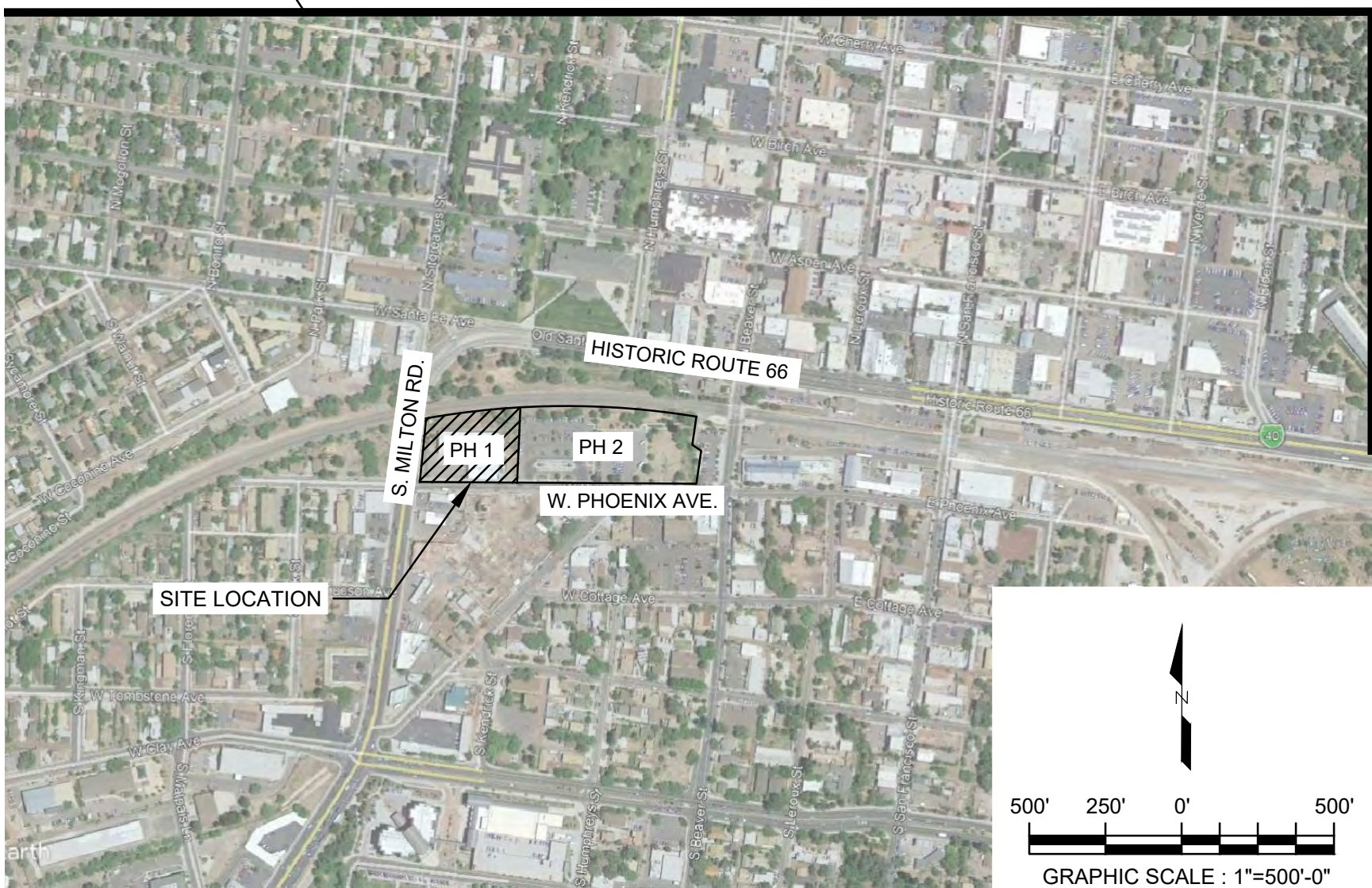
CITY OF FLAGSTAFF
211 W. ASPEN AVENUE
FLAGSTAFF, AZ 86001
(928) 637-8384
CONTACT: BRYCE DOTY

DEVELOPER

MOUNTAIN LINE
3773 N. KASPER DRIVE
FLAGSTAFF, AZ 86004
(928) 679-2423
CONTACT: KATE MORLEY

PREPARER

AECOM
7720 N. 16TH STREET, SUITE 100
PHOENIX, AZ 85020
(602) 371-1100
CONTACT: JENNIFER LOVE



VICINITY MAP

PROJECT INFORMATION

DEVELOPMENT NAME:	DOWNTOWN CONNECTION CENTER (DCC) - PHASE 1
SITE ADDRESS:	216 W. PHOENIX AVENUE, FLAGSTAFF, AZ 86001
ASSESSOR'S PARCEL NUMBER (APN):	100-43-003B
PARCEL SIZE - GROSS:	0.92 AC (39,995 SF)
PARCEL SIZE - NET (LESS BLDG FOOTPRINT):	0.55 AC (24,074 SF)
ZONE DISTRICT:	CS
ZONING DESCRIPTION:	COMMERCIAL SERVICE
LIGHTING ZONE:	2
PARKING ZONE:	ES
TRANSECT ZONE:	T5 MAIN STREET
PROJECT TYPE:	COMMERCIAL
FAR - PERMITTED BY ZONE DISTRICT:	2.0
FAR - PROPOSED:	0.47

BUILDING INFORMATION

GROSS FLOOR AREA:	18,983 SF
NUMBER OF FLOORS:	2
PERMITTED MAXIMUM HEIGHT BY ZONE DISTRICT:	48 FEET
PROPOSED MAXIMUM HEIGHT:	44 FEET
NUMBER OF UNITS:	N/A
PROPOSED COMMERCIAL SUITE NUMBERS:	N/A
BUILDING SQUARE FOOTAGE - FIRST FLOOR:	10,447 SF
BUILDING SQUARE FOOTAGE - SECOND FLOOR:	8,536 SF
PROPOSED IBC OCCUPANCY CLASS:	A AND B
PROPOSED IBC CONSTRUCTION TYPE:	2B
PROPOSED FIRE SPRINKLERS:	YES

PARKING INFORMATION

REQUIRED OFF-STREET PARKING SPACES:	5*	
PROPOSED OFF-STREET PARKING SPACES:	5	
REQUIRED OFF-STREET ADA SPACES:	1	(1 PER 20 PARKING SPACES)
PROPOSED OFF-STREET ADA COMPLIANT PARKING SPACES:	2	
REQUIRED BICYCLE PARKING SPACES:	2	(GREATER OF 5% OF 5 SPACES OR 2 SPACES)
PROPOSED BICYCLE PARKING SPACES:	5	

*PER MEMO ENTITLED "MOUNTAIN LINE DCC PARKING STUDY"
SUBMITTED AS PART OF THIS SITE PLAN SUBMITTAL PACKAGE.

SHEET INDEX

- COVER SHEET
- PHASING PLAN
- SITE PLAN - PHASE 1
- SITE PLAN - PHASE 1 - DETAILS - BIKE RACK
- BUILDING FIRST FLOOR PLAN
- BUILDING SECOND FLOOR PLAN
- BUILDING ELEVATIONS
- BUILDING ELEVATIONS
- BUILDING PERSPECTIVE VIEW
- BUILDING PERSPECTIVE VIEW
- BUILDING MATERIALS
- LANDSCAPE PLAN - PHASE 1
- GRADING AND DRAINAGE PLAN - PHASE 1



AECOM



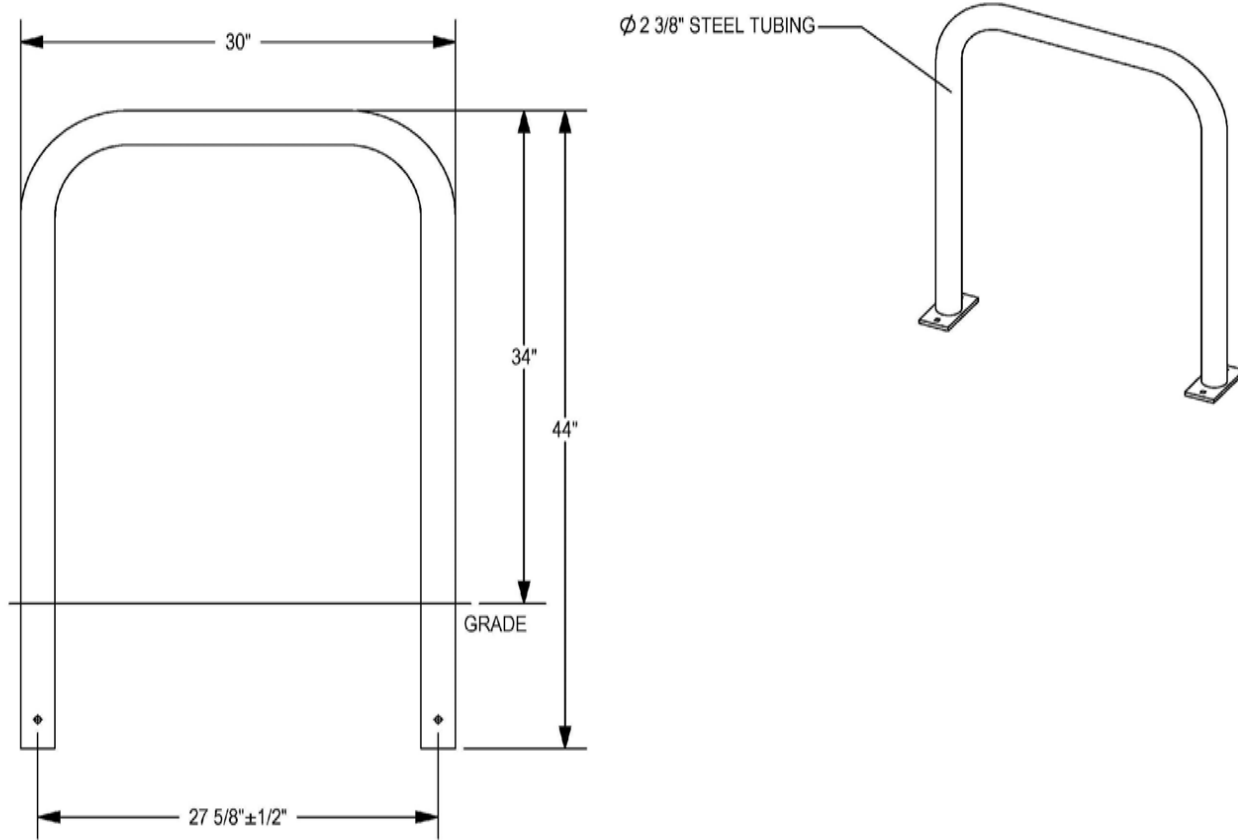
DWG File: C:\Users\McCue\AECOM\NAIPTA DCC - General\Landscape Arch\900_CAD_GIS\910_CAD\Sheets\60595394_DCC_Site_Plan.dwg
McCue, Jodi
15:56
4-Nov-2021

SITE PLAN - BIKE RACK DETAIL

			Submittal:	Seal:	Consultant:	Client:	Designed By:	NAIPTA MOUNTAIN LINE DOWNTOWN CONNECTION CENTER (DCC) - PHASE ONE 216 W. PHOENIX AVENUE, FLAGSTAFF, AZ 86001 ASSESSOR'S PARCEL NUMBER (APN) 1004405A, 10043003B, 10043001D PROPERTY OWNER: BRYCE DOTY, CITY OF FLAGSTAFF, (928) 637-8384 DEVELOPER: KATE MORLEY, MOUNTAIN LINE, (928) 679-8903 3773 N. KASPER DRIVE, FLAGSTAFF, AZ 86004 PREPARER: JENNIFER LOVE, AECOM, (602) 648-2423	@ 24X36 CADD Filename: 60595394_DCC_Site_Plan.dwg Submittal Date: 11/9/2021 Contract No.: 60595394 Sheet No.: 3
			SITE PLAN	PRELIMINARY NOT FOR CONSTRUCTION	<div><div><div><div></div><div>AECOM</div></div><div>7720 North 16th Street Suite 100 Phoenix, AZ 85020 Phone: (602) 371-1100</div></div><div>Submitted By: _____</div></div>	<div><div><div><div></div><div>Northern Arizona Intergovernmental</div><div>NAIPTA</div><div>Public Transportation Authority</div></div><div><div>MOUNTAIN LINE</div></div></div></div>	JM		
							KB/JM		
							CL		
REV	DATE	DESCRIPTION					Approved By: _____		



MADRAX DIVISION
GRABER MANUFACTURING, INC.
1080 UNIKO DRIVE
WALUNAKEE, WI 53597
P(800) 448-7831, P(800) 848-1082, F(800) 848-1081
WWW.MADRAX.COM, E-MAIL: SALES@MADRAX.COM



PRODUCT: UX238-IG(SF)
DESCRIPTION: 1/2 BIKE RACK
DATE: 10-5-18
ENG: SMC
CONFIDENTIAL DRAWING AND INFORMATION IS NOT TO BE COPIED OR DISCLOSED TO OTHERS WITHOUT THE CONSENT OF GRABER MANUFACTURING, INC. SPECIFICATIONS ARE SUBJECT TO CHANGE WITHOUT NOTICE.
©2018 GRABER MANUFACTURING, INC. ALL PROPRIETARY RIGHTS RESERVED.

NOTES:
1. INSTALL BIKE RACKS ACCORDING TO MANUFACTURER'S SPECIFICATIONS.
2. CONSULTANT TO SELECT COLOR/FINISH; SEE MANUFACTURER'S SPECIFICATIONS.
3. SEE SITE PLAN FOR LOCATION OR CONSULT OWNER.

BIKE RACK



S. MILTON ROAD

W. PHOENIX AVENUE

Call at least two full working days before you begin excavation.

Arizona Blue Stake, Inc.
Dial 8-1-1 or 1-800-STAKE-IT (782-5348)
in Maricopa County: (602) 263-1100

11/8/2021 2:12:36 PM

REV	DATE	DESCRIPTION

Submittal:

SITE PLAN

Seal:

**PRELIMINARY
NOT FOR
CONSTRUCTION**

Consultant:
AECOM
7720 North 16th Street
Suite 100
Phoenix, AZ 85020
Phone: (602) 371-1100

Submitted By: _____

Client:



Approved By: _____

Designed By:
PA
Drawn By:
PA
Checked By:
NM, AH, TM
Approved By:
Approver

**NAIPTA MOUNTAIN LINE
DOWNTOWN CONNECTION CENTER (DCC) - PHASE ONE**
216 W. PHOENIX AVENUE, FLAGSTAFF, AZ 86001
ASSESSOR'S PARCEL NUMBER (APN): 1004405A, 10043003B, 10043001D
PROPERTY OWNER: BRYCE DOTY, CITY OF FLAGSTAFF, (928) 637-8384
DEVELOPER: KATE MORLEY, MOUNTAIN LINE, (928) 679-8903
3773 N. KASPER DRIVE, FLAGSTAFF, AZ, 86004
PREPARER: JENNIFER LOVE, AECOM (602) 648-2423

@ 24X36
Title:
First Floor Plan
Submittal Date:
11/09/2021
Contract No.:
TBD
Sheet No.:
4





1 Second Floor
1/8" = 1'-0"

Call at least two full working days
before you begin excavation.

Arizona Blue Stake, Inc.
Dial 8-1-1 or 1-800-STAKE-IT (782-5348)
in Maricopa County: (602) 263-1100

11/8/2021 2:10:59 PM

REV	DATE	DESCRIPTION

Submittal:

SITE PLAN

Seal:

**PRELIMINARY
NOT FOR
CONSTRUCTION**

Consultant:

AECOM

7720 North 16th Street
Suite 100
Phoenix, AZ 85020
Phone: (602) 371-1100

Submitted By: _____

Client:

NAIPTA
Public Transportation Authority

MOUNTAIN LINE

Approved By: _____

Designed By:
PA

Drawn By:
PA

Checked By:
NM, AH, TM

Approved By:
Approver

**NAIPTA MOUNTAIN LINE
DOWNTOWN CONNECTION CENTER (DCC) - PHASE ONE**

216 W. PHOENIX AVENUE, FLAGSTAFF, AZ 86001
ASSESSOR'S PARCEL NUMBER (APN): 1004405A, 10043003B, 10043001D

PROPERTY OWNER: BRYCE DOTY, CITY OF FLAGSTAFF, (928) 637-8384
DEVELOPER: KATE MORLEY, MOUNTAIN LINE, (928) 679-8903
3773 N. KASPER DRIVE, FLAGSTAFF, AZ, 86004
PREPARER: JENNIFER LOVE, AECOM (602) 648-2423

@ 24X36

Title:
Second Floor Plan

Submittal Date:
11/09/2021

Contract No.:
TBD

Sheet No.:
5

NOTES



- STONE VENEER
- BOARD FORM CONCRETE
- METAL PANEL
- STANDING SEAM METAL PANEL ROOF



FIRST FLOOR = 6900.70

Call at least two full working days before you begin excavation.

Arizona Blue Stake, Inc.
Dial 8-1-1 or 1-800-STAKE-IT (782-5348)
in Maricopa County: (602) 263-1100

11/8/2021 2:09:18 PM

REV	DATE	DESCRIPTION

Submittal:
SITE PLAN

Seal:
PRELIMINARY NOT FOR CONSTRUCTION

Consultant:
AECOM
7720 North 16th Street Suite 100 Phoenix, AZ 85020 Phone: (602) 371-1100
Submitted By: _____

Client:
 
Approved By: _____

Designed By:
PA
Drawn By:
PA
Checked By:
NM, AH, TM
Approved By:
Approver

NAIPTA MOUNTAIN LINE DOWNTOWN CONNECTION CENTER (DCC) - PHASE ONE
216 W. PHOENIX AVENUE, FLAGSTAFF, AZ 86001
ASSESSOR'S PARCEL NUMBER (APN): 1004405A, 10043003B, 10043001D
PROPERTY OWNER: BRYCE DOTY, CITY OF FLAGSTAFF, (928) 637-8384
DEVELOPER: KATE MORLEY, MOUNTAIN LINE, (928) 679-8903
3773 N. KASPER DRIVE, FLAGSTAFF, AZ, 86004
PREPARER: JENNIFER LOVE, AECOM (602) 648-2423

@ 24X36
Title: Building Elevations
Submittal Date: 11/09/2021
Contract No.: TBD
Sheet No.: 6

NOTES



2 EAST - ENTRANCE ELEVATION
1/8" = 1'-0"

- STONE VENEER
- BOARD FORM CONCRETE
- METAL PANEL
- STANDING SEAM METAL PANEL ROOF



1 S. MILTON RD
1/8" = 1'-0"

FIRST FLOOR = 6900.70

Call at least two full working days
before you begin excavation.

Arizona Blue Stake, Inc.
Dial 8-1-1 or 1-800-STAKE-IT (782-5348)
in Maricopa County: (602) 263-1100

11/8/2021 1:55:24 PM

REV	DATE	DESCRIPTION

Submittal:
SITE PLAN

Seal:
PRELIMINARY NOT FOR CONSTRUCTION

Consultant:
AECOM
7720 North 16th Street Suite 100 Phoenix, AZ 85020 Phone: (602) 371-1100
Submitted By: _____

Client:
 
Approved By: _____

Designed By:
PA
Drawn By:
PA
Checked By:
NM, AH, TM
Approved By:
Approver

NAIPTA MOUNTAIN LINE DOWNTOWN CONNECTION CENTER (DCC) - PHASE ONE
216 W. PHOENIX AVENUE, FLAGSTAFF, AZ 86001
ASSESSOR'S PARCEL NUMBER (APN): 1004405A, 10043003B, 10043001D
PROPERTY OWNER: BRYCE DOTY, CITY OF FLAGSTAFF, (928) 637-8384
DEVELOPER: KATE MORLEY, MOUNTAIN LINE, (928) 679-8903
3773 N. KASPER DRIVE, FLAGSTAFF, AZ, 86004
PREPARER: JENNIFER LOVE, AECOM (602) 648-2423

@ 24X36
Title: Building Elevations
Submittal Date: 11/09/2021
Contract No.: TBD
Sheet No.: 7



Call at least two full working days
before you begin excavation.

Arizona Blue Stake, Inc.
Dial 8-1-1 or 1-800-STAKE-IT (782-5348)
in Maricopa County: (602) 263-1100

11/8/2021 1:53:04 PM

REV	DATE	DESCRIPTION

Submittal:
SITE PLAN

Seal:
PRELIMINARY NOT FOR CONSTRUCTION

Consultant:
AECOM
7720 North 16th Street Suite 100 Phoenix, AZ 85020 Phone: (602) 371-1100
Submitted By: _____

Client:
 
Approved By: _____

Designed By:
PA
Drawn By:
PA
Checked By:
NM, AH, TM
Approved By:
NM, AH

NAIPTA MOUNTAIN LINE DOWNTOWN CONNECTION CENTER (DCC) - PHASE ONE
216 W. PHOENIX AVENUE, FLAGSTAFF, AZ 86001 ASSESSOR'S PARCEL NUMBER (APN): 1004405A, 10043003B, 10043001D PROPERTY OWNER: BRYCE DOTY, CITY OF FLAGSTAFF, (928) 637-8384 DEVELOPER: KATE MORLEY, MOUNTAIN LINE, (928) 679-8903 3773 N. KASPER DRIVE, FLAGSTAFF, AZ, 86004 PREPARER: JENNIFER LOVE, AECOM (602) 648-2423

@ 24X36
Title: Perspective View
Submittal Date: 11/09/2021
Contract No.: TBD
Sheet No.: 8



Call at least two full working days before you begin excavation.

Arizona Blue Stake, Inc.
Dial 8-1-1 or 1-800-STAKE-IT (782-5348)
in Maricopa County: (602) 263-1100

11/8/2021 1:50:52 PM

REV	DATE	DESCRIPTION

Submittal:
SITE PLAN

Seal:
PRELIMINARY NOT FOR CONSTRUCTION

Consultant:
AECOM
7720 North 16th Street Suite 100 Phoenix, AZ 85020 Phone: (602) 371-1100
Submitted By: _____

Client:
 
Approved By: _____

Designed By:
PA
Drawn By:
PA
Checked By:
NM, AH, TM
Approved By:
Approver

NAIPTA MOUNTAIN LINE DOWNTOWN CONNECTION CENTER (DCC) - PHASE ONE
216 W. PHOENIX AVENUE, FLAGSTAFF, AZ 86001
ASSESSOR'S PARCEL NUMBER (APN): 1004405A, 10043003B, 10043001D
PROPERTY OWNER: BRYCE DOTY, CITY OF FLAGSTAFF, (928) 637-8384
DEVELOPER: KATE MORLEY, MOUNTAIN LINE, (928) 679-8903
3773 N. KASPER DRIVE, FLAGSTAFF, AZ, 86004
PREPARER: JENNIFER LOVE, AECOM (602) 648-2423

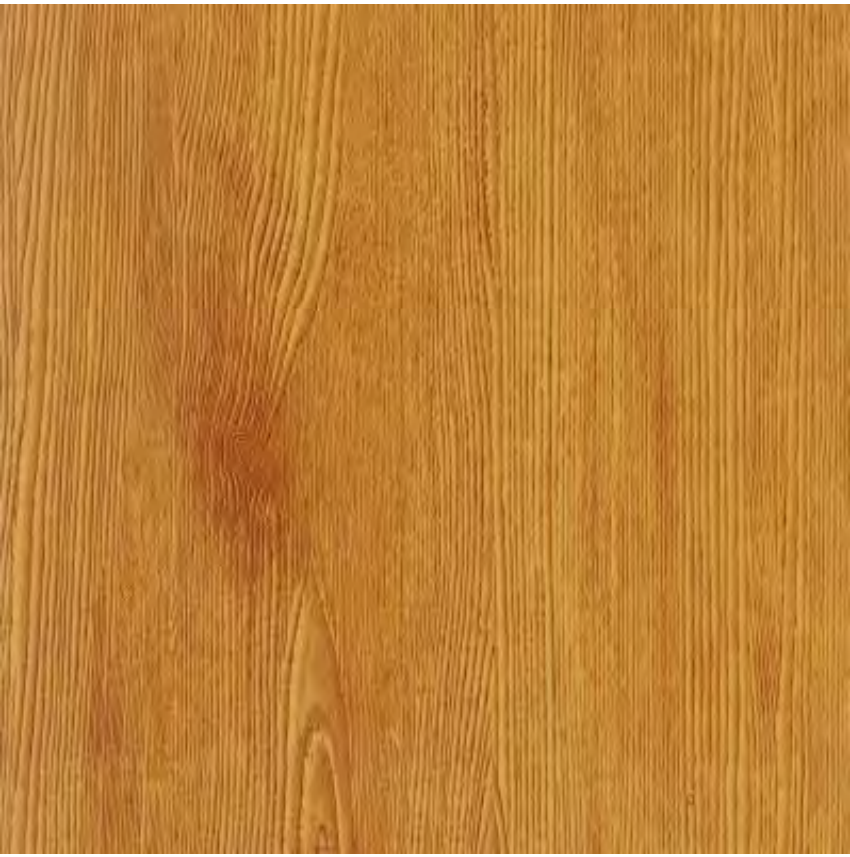
@ 24X36
Title: Perspective View
Submittal Date: 11/09/2021
Contract No.: TBD
Sheet No.: 9



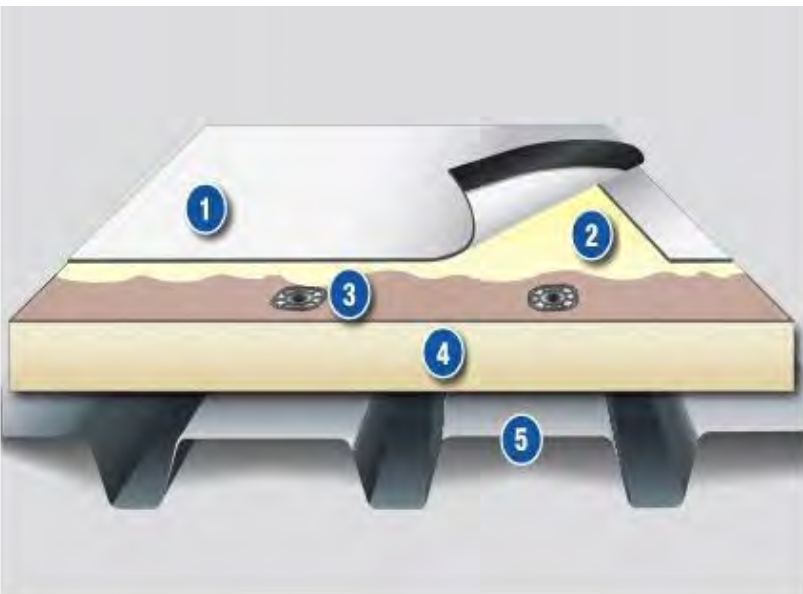
STONE VENEER
MANUF: Stone Solutions Arizona
PRODUCT: STONE VENEER
SIZE:
COLOR: BEAR HOLLOW DRY STACK
REFLECTANCE: SRI 38, LRV APPROX. 17



STANDING SEAM METAL PANEL ROOF
MANUF: FIRESTONE
PRODUCT: UNACLAD - UC-3
- .040" ALUMINUM
- 1.5" RIB, 12" WIDE PANEL
- FLUOROPOLYMER COATING
COLOR: CHARCOAL GRAY
REFLECTANCE: SRI 29, LRV APPROX. 7



WOOD LOOK METAL SOFFIT
MANUF: KNOTWOOD
PRODUCT: SOFFIT SYSTEM
- .055" ALUMINUM
- 1.5" RIB, 6" WIDE PANEL
COLOR: KNOTTY PINE
REFLECTANCE: SRI 29, LRV APPROX. 32



EPDM ROOF
MANUF: CARLISLE
PRODUCT: SURE-WHITE EPDM
TYPE: FULLY ADHERED
SIZE: 60 MIL, 10' WIDE
COLOR: WHITE
REFLECTANCE: SRI 99, LRV APPROX. 84



METAL PANEL
MANUF: CENTRIA
PRODUCT: INTERCEPT HLZ
- .060" ALUMINUM
- 1 3/8" DEEP
- 3/4" REVEAL
- SUNDANCE AM ALUM. METALLIC 3-COAT
COLOR: 1243 MOCHA
REFLECTANCE: SRI 43, LRV APPROX. 22



CURTAINWALL GLAZING SYSTEM
MANUF: KAWNEER
PRODUCT: 1620UT CURTAIN WALL SYSTEM
- DOUBLE GLAZED INSULATED GLASS
- 2" SIGHTLINE & 7-1/2" DEPTH
- 12" MULLION EXTENSION AT LOBBY
COLOR: CHARCOAL
REFLECTANCE: SRI 43, LRV APPROX. 22

GLASS
MANUF: VIRACON
PRODUCT: VE1-2M
- 1/4" CLEAR HS
- 1/2" BLACK SPACER
- 1/4" CLEAR HS
VLT: 59%
SHGC: 0.33
REF: 16%
SHADING: 0.38



CAST IN PLACE CONCRETE WITH BOARD FORMS
MANUF: FITZGERALD FORMLINERS
PRODUCT: 16020 ROUGH GRAIN PLANK
SIZE: 4.3" BOARD WIDTH
COLOR: REGULAR CONCRETE
REFLECTANCE: SRI 45, LRV APPROX. 24

Call at least two full working days
before you begin excavation.

Arizona Blue Stake, Inc.
Dial 8-1-1 or 1-800-STAKE-IT (782-5348)
in Maricopa County: (602) 263-1100

11/8/2021 1:42:56 PM

REV	DATE	DESCRIPTION

Submittal:
SITE PLAN

Seal:
PRELIMINARY NOT FOR CONSTRUCTION

Consultant:
AECOM
7720 North 16th Street Suite 100 Phoenix, AZ 85020 Phone: (602) 371-1100
Submitted By: _____

Client:
 
Approved By: _____

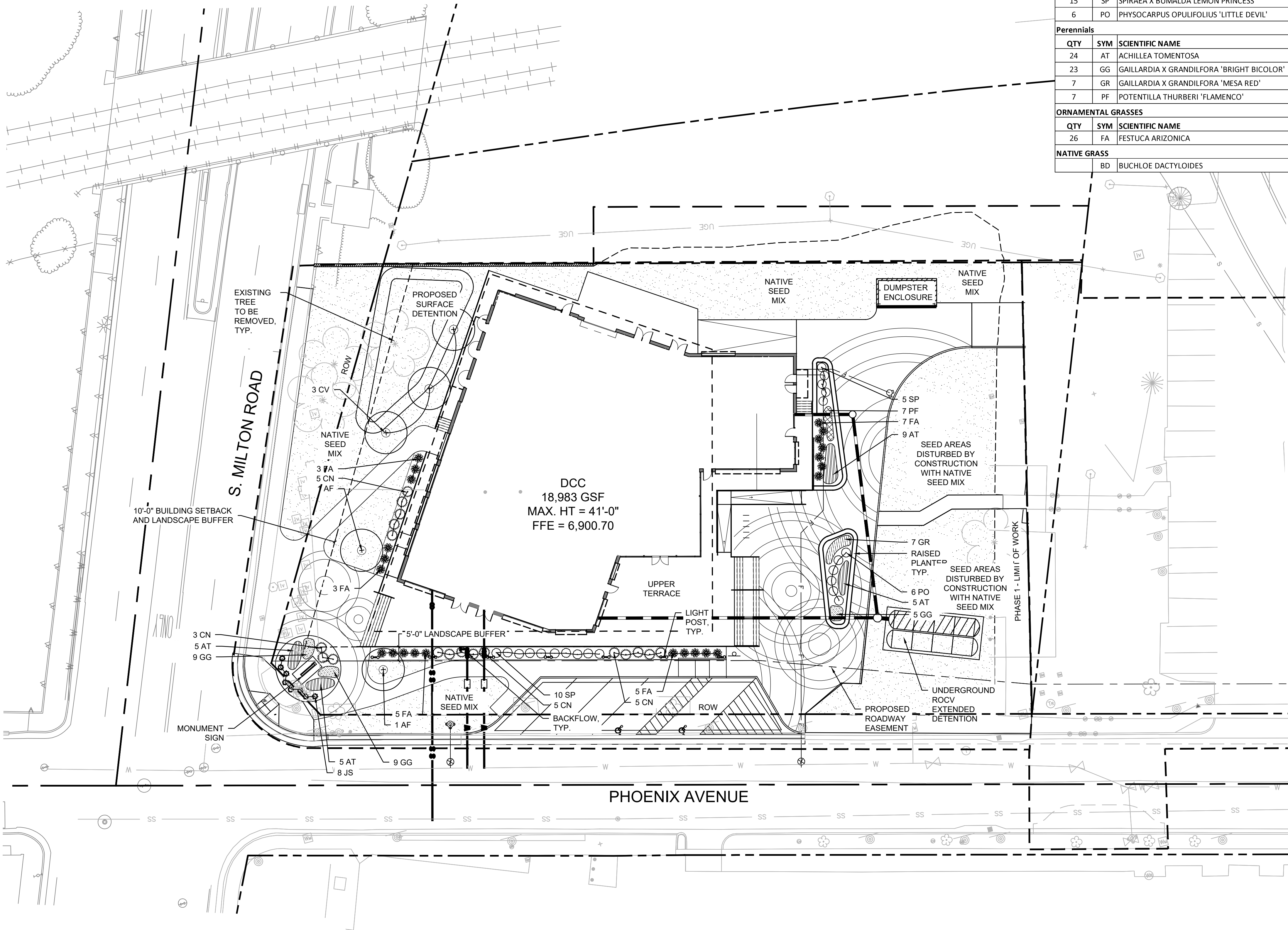
Designed By:
Designer
Drawn By:
Author
Checked By:
Checker
Approved By:
Approver

NAIPTA MOUNTAIN LINE DOWNTOWN CONNECTION CENTER (DCC) - PHASE ONE
216 W. PHOENIX AVENUE, FLAGSTAFF, AZ 86001
ASSESSOR'S PARCEL NUMBER (APN): 1004405A, 10043003B, 10043001D
PROPERTY OWNER: BRYCE DOTY, CITY OF FLAGSTAFF, (928) 637-8384
DEVELOPER: KATE MORLEY, MOUNTAIN LINE, (928) 679-8903
3773 N. KASPER DRIVE, FLAGSTAFF, AZ, 86004
PREPARER: JENNIFER LOVE, AECOM (602) 648-2423

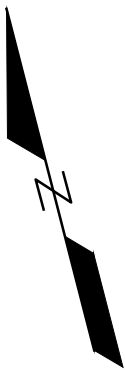
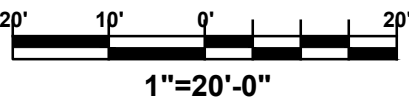
@ 24X36
Title:
Material Specification
Submittal Date:
11/09/2021
Contract No.:
TBD
Sheet No.:
10

DWG File: C:\Users\McCue\OneDrive\Documents\AECOM\NAIPTA DCC - General Landscape Arch\900_CAD_Sheets\0595394_DCC_Landscape_Plan.dwg
McCue, Jodi

15:38
4-Nov-2021



PHASE ONE - DCC PLANTING SCHEDULE							
Trees							
QTY	SYM	SCIENTIFIC NAME	COMMON NAME	SIZE	CONDITION	COMMENTS	NATIVE
2	AF	ACER X FREEMANII 'ARMSTRONG'	ARMSTRONG MAPLE	2" CAL.	B&B	SPACING AS PER PLAN	NATURALIZED
Ornamental Trees							
QTY	SYM	SCIENTIFIC NAME	COMMON NAME	SIZE	CONDITION	COMMENTS	
3	CV	CRATAEGUS VIRIDIS 'WINTER KING'	WINTER KING HAWTHORN	2" CAL.	B&B	SPACING AS PER PLAN	NATURALIZED
Shrubs							
QTY	SYM	SCIENTIFIC NAME	COMMON NAME	SIZE	CONDITION	COMMENTS	
13	CN	CHRYSOETHAMNUS NAUSEOSUS VAR. NAUSEOSUS	DWARF BLUE RABBITBUSH			SPACING AS PER PLAN	NATIVE
8	JS	JUNIPERUS SQUMAMATUS 'BLUE STAR'	BLUE STAR JUNIPER	18"	CONT.	SPACING AS PER PLAN	NATURALIZED
15	SP	SPIRAEA X BUMALDA LEMON PRINCESS'	LEMON PRINCESS SPIRAEA	18"	CONT.	SPACING AS PER PLAN	NATURALIZED
6	PO	PHYSOCARPUS OPULIFOLIUS 'LITTLE DEVIL'	LITTLE DEVIL NINEBARK	18"	CONT.	SPACING AS PER PLAN	NATIVE
Perennials							
QTY	SYM	SCIENTIFIC NAME	COMMON NAME	SIZE	CONDITION	COMMENTS	
24	AT	ACHILLEA TOMENTOSA	WOOLY YARROW	NO. 1	CONT.	24 IN. SPACING; CLUMP	NATIVE
23	GG	GAILLARDIA X GRANDIFLORA 'BRIGHT BICOLOR'	BRIGHT BICOLOR BLANKET FLOWER	NO. 1	CONT.	12 IN. SPACING; CLUMP	NATIVE
7	GR	GAILLARDIA X GRANDIFLORA 'MESA RED'	MESA RED BLANKET FLOWER	NO. 1	CONT.	24 IN. SPACING; CLUMP	NATIVE
7	PF	POTENTILLA THURBERI 'FLAMENCO'	FLAMENCO CINQUOIL	NO. 1	CONT.	24 IN. SPACING; CLUMP	NATIVE
ORNAMENTAL GRASSES							
QTY	SYM	SCIENTIFIC NAME	COMMON NAME	SIZE	CONDITION	COMMENTS	
26	FA	FESTUCA ARIZONICA	ARIZONA FESCUE	NO. 1	CONT.	SPACING AS PER PLAN	NATIVE
NATIVE GRASS							
	BD	BUCHLOE DACTYLOIDES	BUFFALO GRASS	SOD			NATIVE



LANDSCAPE PLAN - PHASE 1

--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--

DRAFT Mountain Line DCC – Cultural Resources Study

Nov 9, 2021

The enclosed **Cultural Resources Survey for the Mountain Line Downtown Connection Center, Flagstaff, Coconino County, Arizona** is a working draft. It is a deliberative document and review by Northern Arizona Intergovernmental Public Transportation Authority (NAIPTA) and the Federal Transit Administration (FTA) is pending. This draft is provided to support the site plan application process with the City of Flagstaff. The FTA, in coordination with NAIPTA, will undertake Section 106 consultation for the project. A finalized draft will be provided to consulting parties to support the 106 process, once initiated. Project information, data, eligibility determinations, and finding of effect recommendations in the report are subject to change as the environmental review process progresses.

Cultural Resources Survey for the Mountain Line Downtown Connection Center, Flagstaff, Coconino County, Arizona

Prepared for

Northern Arizona Intergovernmental Public Transportation Authority
(NAIPTA)

Prepared by

AECOM

Restricted Distribution:

Restrict distribution of information in this document about the location, character, or ownership of archaeological and historic resources if that information could result in significant invasion of privacy, damage to the archaeological and historical resources, or impede the use of traditional religious sites by practitioners, in accordance with applicable regulatory requirements, including Section 304 of the National Historic Preservation Act, Section 9(a) of the Archaeological Resources Protection Act, and Section 39-125 of the Arizona Revised Statutes.

Cultural Resource Report 2021-12(AZ)

September 2021

Prepared for:

Northern Arizona Intergovernmental Public Transportation Authority

Prepared by:

Kirsten Johnson and A.E. (Gene) Rogge

T: 602-861-7413, 602-861-7414

E: kirsten.johnson@aecom.com, gene.rogge@aecom.com

AECOM

7720 North 16th Street

Phoenix, AZ 85020

aecom.com

Restrict Distribution

Restrict distribution of information in this document about the location, character, or ownership of archaeological and historic resources if that information could result in significant invasion of privacy, damage to the archaeological and historical resources, or impede the use of traditional religious sites by practitioners, in accordance with applicable regulatory requirements, including Section 304 of the National Historic Preservation Act, Section 9(a) of the Archaeological Resources Protection Act, and Section 39-125 of the Arizona Revised Statutes.

Disclaimer

In preparing this report, AECOM used background information compiled by prior studies. AECOM relied on this information as furnished and is neither responsible for nor has confirmed the accuracy of this information.

AECOM Cultural Resource Report 2021-12(AZ)

September 2021

Table of Contents

Abstract.....	vi
Introduction	1
Project Description	1
Project Location and Land Ownership.....	5
Regulatory Requirements	5
Area of Potential Effects	7
Project Team	8
Environmental and Cultural Contexts	9
Environmental Setting.....	9
Cultural History Summary.....	9
Prehistoric Period	9
Ethnohistoric and Historic Periods	11
Records Review	15
Review Methods.....	15
Prior Cultural Resource Surveys.....	15
Previously Recorded Cultural Resources	17
Review of Historic Maps	21
Survey Methods and Results	27
Methods	27
Survey Results	27
Assessment of Impacts.....	28
Summary and Recommendations	30
References.....	31
Appendix A: Historic Buildings and Structures within the 0.5-Mile Buffer.....	37
Appendix B: Arizona State Historic Property Inventory Forms.....	39
Appendix C: Photographs	40

Figures

Figure 1. General Project Location.....	2
Figure 2. Concept Plan and Construction Phasing	3
Figure 3. Concept Plan and Rio de Flag Easement.....	4
Figure 4. Project Area and Area of Potential Effects	5
Figure 5. Prior Studies and Previously Recorded Historic In-Use Structures.....	5
Figure 6. Historic and Current Alignments of Rio de Flag in the Project Vicinity	10
Figure 7. Historic-Period Properties in the APE	19
Figure 8. 1879 GLO Plat.....	22
Figure 9. 1892 Sanborn Fire Insurance Map	23
Figure 10. 1916 Sanborn Fire Insurance Map	23
Figure 11. Aerial view of the Project Area in 2020 showing locations of the features mapped on the 1916 and 1948 Sanborn maps.....	25
Figure 12. 1948 Sanborn Fire Insurance Map	25
Figure 13. 1959 aerial photograph.	26

Tables

Table 1. Prior Cultural Resource Studies in the Records Review Area ⁽¹⁾	15
Table 2. Previously Recorded Historic Buildings and Structures in the APE	17
Table 3. Previously Unrecorded Historic-Period Buildings in the APE	28
Table 4. Assessment of Impacts on NRHP-Listed and Eligible Properties	28

Photos

Photo 1. View of the Arizona Mineral Belt Railroad Trestle, looking southeast.....	41
Photo 2. View from the west end of the Arizona Mineral Belt Railroad, looking southwest.....	41
Photo 3. View of the Arizona Machinery Co. Building, the Hutchison & Sauer Building, the Hutchison Building, Marcos Café, looking southwest.	42
Photo 4. View of the Hutchison & Sauer Building, the Hutchison Building, and Marcos Cafe, looking southeast.....	42
Photo 5. View toward the Project area from the Hutchison & Sauer and Hutchison buildings and Marcos Cafe, looking northwest.	43
Photo 6. View of the Coca-Cola Bottling Works, looking south-southwest.....	43
Photo 7. View of the T.E. Pulliam House, looking southeast.	44
Photo 8. View of the Flagstaff Steam Laundry, looking southwest.....	44
Photo 9. View toward the eastern section of the Project area from the vicinity of the Coca-Cola Bottling Works, the T.E. Pulliam House, and Flagstaff Steam Laundry, looking northeast.	45
Photo 10. View toward the central section of the Project area from the vicinity of the Coca-Cola Bottling Works, the T.E. Pulliam House, and Flagstaff Steam Laundry, looking north.....	45
Photo 11. View toward the western section of the Project area from the vicinity of the Coca-Cola Bottling Works, the T.E. Pulliam House, and Flagstaff Steam Laundry, looking northwest.....	46
Photo 12. View of the 5-story apartment building (The Jack) on the south side of W. Phoenix Avenue, looking southwest. The existing DCC is visible in the foreground.....	46
Photo 13. View of the Arizona Central Commercial Company Warehouse, looking southeast.	47
Photo 14. View of the Du Beau Motel, looking west-southwest.....	47
Photo 15. View toward the Project area from the facade of the Arizona Central Commercial Co. Warehouse, looking west.	48
Photo 16. View toward the Project area from the northeast corner of the intersection of S. Beaver St. and W. Phoenix Ave., looking west.	48
Photo 17. View toward the Project area from the southeast corner of the intersection of S. Beaver Street and W. Phoenix Ave., looking northwest.....	49
Photo 18. View of the ATSF and the existing DCC, looking southwest.	49
Photo 19. View of an existing landscaped area and the ATSF north of the existing DCC, looking west....	50
Photo 20. View of the intersection of Route 66/US 89A/US 89 and W. Phoenix Avenue, looking east toward the Project area.....	50

Acronyms and Abbreviations

A&P	Atlantic & Pacific Railroad
ADOT	Arizona Department of Transportation
APE	Area of Potential Effects
APS	Arizona Public Service
ASM	Arizona State Museum
AT&SF	Atchison, Topeka & Santa Fe Railway
BCE	Before the Current Era
CARy	Central Arizona Railway Company
CE	Current Era
DCC	Downtown Connection Center
FTA	Federal Transit Administration
GLO	General Land Office
HAER	Historic American Engineering Record
MRA	Multiple Resource Area
NAIPTA	Northern Arizona Intergovernmental Public Transportation Authority
NEPA	National Environmental Policy Act
NHPA	National Historic Preservation Act
NRHP	National Register of Historic Places
Project	Mountain Line Downtown Connection Center
SHPO	State Historic Preservation Office
USACE	US Army Corps of Engineers

Abstract

Report Title: Cultural Resource Survey for Mountain Line Downtown Connection Center, Flagstaff, Coconino County, Arizona. Draft. September 2021.

Project Name: Downtown Connection Center (DCC)

Project Location: City of Flagstaff

Project Locator UTM: 440605 meters east, 3895132 meters north, Zone 12N, North American Datum 83

Project Sponsor: Northern Arizona Intergovernmental Public Transportation Authority (NAIPTA)

Sponsor Project Number: none

Lead Agency: Federal Transit Administration (FTA)

Other Involved Agencies: City of Flagstaff, Arizona Department of Transportation (ADOT)

Applicable Regulations:

National Environmental Policy Act (NEPA);

Section 106 of the National Historic Preservation Act (NHPA) (54 US Code 300101 et seq.), implemented by regulations for Protection of Historic Properties (Title 36, Code of Federal Regulations, part 800); Section 4(f) of the Department of Transportation Act of 1966 (49 US Code § 303);

Arizona Antiquities Act (Arizona Revised Statutes §§ 41-841 through 41-844;

State Historic Preservation Act (Arizona Revised Statutes §§ 41-861 et seq.);

City of Flagstaff Historic Preservation Ordinance (Chapter 10-15).

Funding Source: federal, state

Description of the Project/Undertaking:

Since 2009, Mountain Line has operated a DCC under an intergovernmental agreement and licensing agreement with the City of Flagstaff. This transit hub, which is on the north side of W. Phoenix Avenue between S. Milton Road and S. Beaver Street, serves approximately 52,000 riders monthly and has approximately 300 buses accessing the site daily. Mountain Line seeks to expand the existing DCC site to improve operational safety and efficiency, improve passenger access, accommodate new 60-foot articulated vehicles, and support expansion of future transit routes. Mountain Line's 2020 Strategic Plan identified a new DCC as the highest priority capital project for supporting public transit service in the community.

The DCC scope includes a multimodal and public transit operations center with 13 bus bays; a 22,000-square foot, two-story public facility that will include Mountain Line customer service, administration and operations; civic space; bicycle and pedestrian infrastructure; and roadway improvements at the W. Phoenix Avenue and S. Milton Road intersection and along W. Phoenix Avenue.

Because the proposed project is on the same site as the US Army Corps of Engineers (USACE) Rio de Flag flood control project, construction of the DCC will be implemented in phases dependent on the Rio de Flag construction schedule.

Phase 1 is located on the western section of the site and is outside the Rio de Flag flood control project limits. Construction is anticipated to be completed in 2023. Phase 1 includes:

- Demolition of an existing building at 216 W. Phoenix Avenue and construction of a new, two-story, DCC facility for Mountain Line rider customer service and amenities, Mountain Line administration, and bus operator comfort facilities. Facility programming includes capacity for partnerships with the City of Flagstaff Police Department as a sub-station, human service agency, and MetroPlan Planning organizational partnerships. The facility will be designed to accommodate future expansion of passenger customer service and amenities, if future projects include Greyhound and Amtrak services.

- Construction of convenience parking spaces along W. Phoenix Avenue, which will be available to the public, pending City of Flagstaff Conditional Use Permit approval. Minimal to no employee parking will be developed as part of Phase 1. Mountain Line may develop a travel demand management policy for employees.
- Construction of a civic plaza space near the building, which will include paved areas, trees for shade, and seating for bus patrons.
- Construction of a temporary pedestrian way to connect the new building to the existing transit center, which would remain in place until the new bus plaza is built during Phase 2.
- Installation of bicycle parking,
- Landscaping of buffer areas between the new building and W. Phoenix Avenue and S. Milton Road.
- Installation of underground storage for on-site drainage.

Phase 2 is located on the central and eastern sections of the site and overlaps with the Rio de Flag flood control project limits. It is anticipated Phase 2 would be constructed after the USACE Rio de Flag flood control project; construction is anticipated to be completed in 2025. Phase 2 includes:

- Construction of a bus plaza with 13 bus bays in two aisles including bus shelters or canopies, pedestrian walkways and crossings providing access to the bus bays, landscaping and shade trees, and space for installation of future amenities and infrastructure to service zero-emission bus technology such as electrical utilities and recharging stations.
- Construction of on-street improvements adjacent to the bus plaza along W. Phoenix Avenue, including a pick-up/drop-off area and loading zone.
- Construction of a civic space on the eastern section of the site, which is anticipated to include pedestrian walkways, seating, shade structures, overhead lighting, and landscaping.
- Construction of a trail along the northern section of the Project area as a component of a planned alignment of the Flagstaff Urban Trail System,
- Installation of pedestrian crossings on W. Phoenix Avenue, which would include Americans with Disabilities Act curb improvements and crosswalk striping at a new midblock crossing and at Mikes Pike.
- Installation of bicycle parking.

Additional roadway improvements in the Project area also are being considered which may be constructed between Phase 1 and Phase 2. These improvements include installation of signals and a pedestrian crossing at the S. Milton Road/W. Phoenix Avenue intersection and construction of a right-turn lane from westbound Phoenix Avenue to northbound Milton Road. The Project would not require the construction of any off-site maintenance facilities, and it is anticipated that construction staging and storage areas and temporary relocation of the transit hub during construction would be confined to the Project area. Construction traffic is projected to add only a minor increment to the existing traffic volumes on adjacent roads.

Project Area/Area of Potential Effects:

The proposed Project is located at 116 and 216 W. Phoenix Avenue in Coconino County, southwest of downtown Flagstaff. The existing DCC encompasses approximately 0.5 acre on multiple parcels located south of downtown Flagstaff. The area proposed for expansion includes multiple Coconino County Assessor's parcels that are currently developed as the existing Mountain Line building, bus facility, and parking lots. The Project is within an urban area surrounded by commercial, residential, and transportation land uses. It is bounded by the BNSF Railway to the north, W. Phoenix Avenue to the south, S. Beaver Street to the east, and S. Milton Road to the west. The land within the Project area is owned by the City of Flagstaff and the BNSF Railway.

Regulations for Protection of Historic Properties, which implement NHPA Section 106 define the area of potential effects (APE) as "the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties" (36 CFR 800.16[d]). Delineation of

the APE for both Phases 1 and 2 considered the nature, scale, and location of the DCC project and the potential of the project to affect historic properties directly and indirectly. In accordance with guidance of the Advisory Council on Historic Preservation (2019), direct effects were considered to occur at the time and place of project implementation with no intervening cause, and indirect effects to occur at a later time and further removed in distance but still reasonably foreseeable. Construction activities within the Project are have the most potential to physically disturb or destroy historic properties (including archaeological resources), but visual or noise impacts could also diminish the integrity of the setting of nearby historic properties and changes in access could also impact their use.

Because the City of Flagstaff is a CLG we coordinated with Mark Reavis, the Flagstaff Heritage Preservation Officer, in delineating the APE. The APE was delineated to include the Project area where the ground could be disturbed by construction activities, including construction staging areas, and encompasses approximately 5.8 acres mostly between W. Phoenix Avenue on the south, the BNSF Railway right-of-way on the north, S. Milton Road to the west, and the west side of S. Beaver Street to the east. Ongoing design indicates the depth of disturbance resulting from construction of the new building, bus shelters, and other facilities; installation or relocation of buried utilities; and potential widening of W. Phoenix Avenue would be approximately 1.5 and 6 feet deep in the vicinity of the new building and underground stormwater storage tanks, and approximately 2 feet throughout the remainder of the site.

The APE was delineated to also include adjacent parcels of land, as defined by the Coconino County Assessor, adjacent to the Project area. Only part of a long parcel owned by the BNSF Railway extending along the railroad tracks far to the west of the Project area was included. The delineation resulted in an irregularly shaped above APE, but that seems appropriate because property parcels would typically serve as boundaries for any historic properties that might be affected. The APE covers 20.9 acres. Although the Project would include a two story building, sight lines toward the Project diminish rapidly beyond the first tier of adjacent parcels because of the built environment and existing landscaping. No potential for visual and auditory impacts or changes in access were identified beyond the delineated APE. The assessment identified no potential for indirect effects that might occur at a later time and at greater distances.

Legal Description:

The APE includes parts of the SE1/4 of Section 16, Township 21 North, Range 7 East, Gila and Salt River Meridian. The area is mapped on the Flagstaff West, Arizona, US Geological Survey 7.5-minute topographic quadrangle.

Land Jurisdiction: City of Flagstaff, BNSF Railway, ADOT right-of-way, private

Total Acres: 20 in the APE

Acres Surveyed: Historic buildings and structures were inventoried within the 20 acres of the APE.

Acres Not Surveyed: No archaeological survey was conducted because no natural ground surface is visible due to pavement, landscaping, or other disturbance.

Consultant Firm/Organization: AECOM

Project Number: AECOM Job 60595394, Cultural Resource Report 2021-12(AZ)

Permit Number(s): none required

Dates of Fieldwork: 12 August 2021

Number of Isolated Occurrences Recorded: 0

Built Environment Activity: intensive survey

Number of Buildings Recorded: 5

Eligible Historic Districts and Buildings:

Two previously listed historic districts (Railroad Addition and Flagstaff Southside) with 9 contributing buildings

Ineligible Historic Districts and Buildings: 5 previously unrecorded

Unevaluated Historic Districts and Buildings: 0

Eligible Historic In-Use Structures: 4 previously recorded (3 highways and 1 railroad trestle)

Ineligible Historic In-Use Structures: Two previously recorded (Flagstaff Underpass and Phoenix Avenue Bridge)

Unevaluated Historic In-Use Structures: 0

Sites Not Relocated: 0

Historic Districts, Buildings, and In-Use Structures Summary Table:

Land Jurisdiction	Identification Status	Name/Number	Eligibility Status/Criterion	Recommended Treatment
Historic Districts				
multiple	previously recorded	Railroad Addition Historic District	listed, Criteria A and C	none
multiple	previously recorded	Flagstaff Southside Historic District	listed, Criteria A and C	none
District Contributors				
private	previously recorded	Arizona Central Commercial Warehouse	listed as contributor to the Railroad Addition Historic District	none
private	previously recorded	Arizona Machinery Co. Building	listed as contributor to the Flagstaff Southside Historic District	none
private	previously recorded	Coca-Cola Bottling Works	listed as contributor to the Flagstaff Southside Historic District	none
private	previously recorded	Du Beau Motel	listed as contributor to the Railroad Addition Historic District	none
private	previously recorded	Flagstaff Steam Laundry	listed as contributor to the Flagstaff Southside Historic District	none
private	previously recorded	Hutchison Building	listed as contributor to the Flagstaff Southside Historic District	none
private	previously recorded	Hutchison & Sauer Building	listed as contributor to the Flagstaff Southside Historic District	none
private	previously recorded	Marcos Café	listed as contributor to the Flagstaff Southside Historic District	none
private	previously recorded	T.E. Pulliam House	recommended eligible as contributor to the Flagstaff Southside Historic District	none
Individual Buildings				
City of Flagstaff	newly recorded	Arizona Distributing Company Warehouse/ML-1	recommended not eligible	none
private	newly recorded	Viotti Furniture/ML-2	recommended not eligible	none
private	newly recorded	Standard Stations, Inc ML-3	recommended not eligible	none
private	newly recorded	Wheeler's Underpass Grocery /ML-4	recommended not eligible	none
private	newly recorded	Flagstaff Chamber of Commerce/ML-5	recommended not eligible	none
In-Use Structures ⁽¹⁾				
private	previously recorded	Atchison, Topeka & Santa Fe Railroad, AZ I:14:334(ASM)	determined eligible, Criteria A and D	none
ADOT	previously recorded	Flagstaff Underpass, ADOT 00529	determined not eligible	none
ADOT	previously recorded	Historic Route 66, AZ I:15:156(ASM)	determined eligible, Criteria A and C	none
ADOT	previously recorded	Prescott-Jerome Highway/US 89A, AZ N:7:61(ASM)	determined eligible, Criterion D	none
City of Flagstaff	previously recorded	Phoenix Avenue Bridge, ADOT 08709	determined not eligible	none
City of Flagstaff	previously recorded	Arizona Mineral Belt Railroad Trestle,	determined eligible, criteria not specified	none

Land Jurisdiction	Identification Status	Name/Number	Eligibility Status/Criterion	Recommended Treatment
ADOT	previously recorded	US Highway 89, AZ 1:3:10(ASM)	determined eligible, Criterion D	none

(1) The Arizona State Museum (ASM) no longer considers in-use historic structures to be archaeological sites and site numbers previously assigned to them are no longer considered valid, except for any abandoned parts of these properties

Comments:

On 12 August 2021, Kirsten Johnson, an AECOM Senior Architectural Historian who meets the Secretary of the Interior Professional Qualification Standards for Historians and Architectural Historians, conducted a field survey of the APE to identify and record historic built environment resources within the APE. The APE was delineated to include the Project area and approximately the adjacent parcels of land, as defined by the Coconino County Assessor. The Project area is southwest of downtown Flagstaff within an urban setting.

Sixteen previously designated historic resources were identified within the APE, including two NRHP-listed historic districts (Flagstaff Southside Historic District and Railroad Addition Historic District); 9 district contributors, 3 historic highways, 1 historic railroad trestle, and 1 historic railroad. Only the historic railroad trestle, an isolated remnant of the Arizona Mineral Belt Railroad, is within the Project area. The USACE Rio de Flag flood control project will likely remove the trestle and the USACE has executed a Memorandum of Agreement stipulating the agency will compile historic documentation and install public signs to mitigate the trestle removal. The current DCC conceptual design indicates the area where the trestle is location would not be modified if the trestle were extant when Phase 2 of the DCC were developed, the Project is unlikely to have an adverse effect on the trestle. The other 15 resources within the APE are outside of the Project area. Although the existing DCC and the proposed Project area are visible or partially visible from most of these resources, the proposed Project would not substantially alter their current viewsheds. Therefore, it is unlikely that the Project would result in any adverse effect on the 16 previously recorded NRHP-listed and eligible resources in the APE.

Five historic-period properties were identified within the APE and recorded on Arizona State Historic Property Inventory forms. All 5 resources were identified as commercial property types constructed between 1938 and 1960. All five historic-period buildings have been altered by additions, substantial design modifications, and/or altered materials. None of the buildings retain integrity of design, materials, workmanship, feeling, or association, and all are recommended ineligible for individual listing in the NRHP. In addition, none of the buildings are within established NRHP-listed or -eligible historic districts and no potential unidentified historic districts were identified.

Introduction

The Federal Transit Administration (FTA), in coordination Northern Arizona Intergovernmental Public Transportation Authority (NAIPTA) proposes to develop the Mountain Line Downtown Connection Center (DCC) by expanding an existing transit facility in Flagstaff, Coconino County, Arizona (Project). Because FTA will be providing financial assistance for the DCC the Project is a federal undertaking. Cultural resource studies were conducted to support a categorical exclusion pursuant to the National Environmental Policy Act (NEPA) and to ensure compliance with Section 106 of the National Historic Preservation Act (NHPA) and other federal, state, and local historic preservation regulations.

Project Description

Mountain Line has operated the existing transit facility since 2009 under an intergovernmental agreement and licensing agreement with the City of Flagstaff. This transit hub, which is on the north side of W. Phoenix Avenue between S. Milton Road and S. Beaver Street (**Figure 1**), serves approximately 52,000 riders monthly and has approximately 300 buses accessing the site daily. Mountain Line seeks to develop the DCC to improve operational safety and efficiency, improve passenger access, accommodate new 60-foot articulated vehicles, and support expansion of future transit routes. Mountain Line's 2020 Strategic Plan identified a new DCC as the highest priority capital project for supporting public transit service in the community.

The planned DCC includes a multimodal and public transit operations center with 13 bus bays; a 22,000-square foot, two-story public facility that will include Mountain Line customer service, administration and operations; civic space; bicycle and pedestrian infrastructure; and roadway improvements at the W. Phoenix Avenue and S. Milton Road intersection and along W. Phoenix Avenue.

Because the proposed project overlaps the corridor of the US Army Corps of Engineers (USACE) Rio de Flag flood control project, construction of the DCC will be implemented in phases dependent on the Rio de Flag construction schedule (**Figures 2 and 3**).

Phase 1 is located on the western section of the site and is outside the Rio de Flag flood control project limits. Construction is anticipated to be completed in 2023. Phase 1 includes:

- Demolition of an existing building at 216 W. Phoenix Avenue and construction of a new, two-story, DCC facility for Mountain Line rider customer service and amenities, Mountain Line administration, and bus operator comfort facilities. Facility programming includes capacity for partnerships with the City of Flagstaff Police Department as a sub-station, human service agency, and MetroPlan Planning organizational partnerships. The facility will be designed to accommodate future expansion of passenger customer service and amenities, if future projects include Greyhound and Amtrak services.
- Construction of convenience parking spaces along W. Phoenix Avenue, which will be available to the public, pending City of Flagstaff Conditional Use Permit approval. Minimal to no employee parking will be developed as part of Phase 1. Mountain Line may develop a travel demand management policy for employees.
- Construction of a civic plaza space near the building, which will include paved areas, trees for shade, and seating for bus patrons.
- Construction of a temporary pedestrian way to connect the new building to the existing transit center, which would remain in place until the new bus plaza is built during Phase 2.
- Installation of bicycle parking,
- Landscaping of buffer areas between the new building and W. Phoenix Avenue and S. Milton Road.
- Installation of underground storage for on-site drainage.

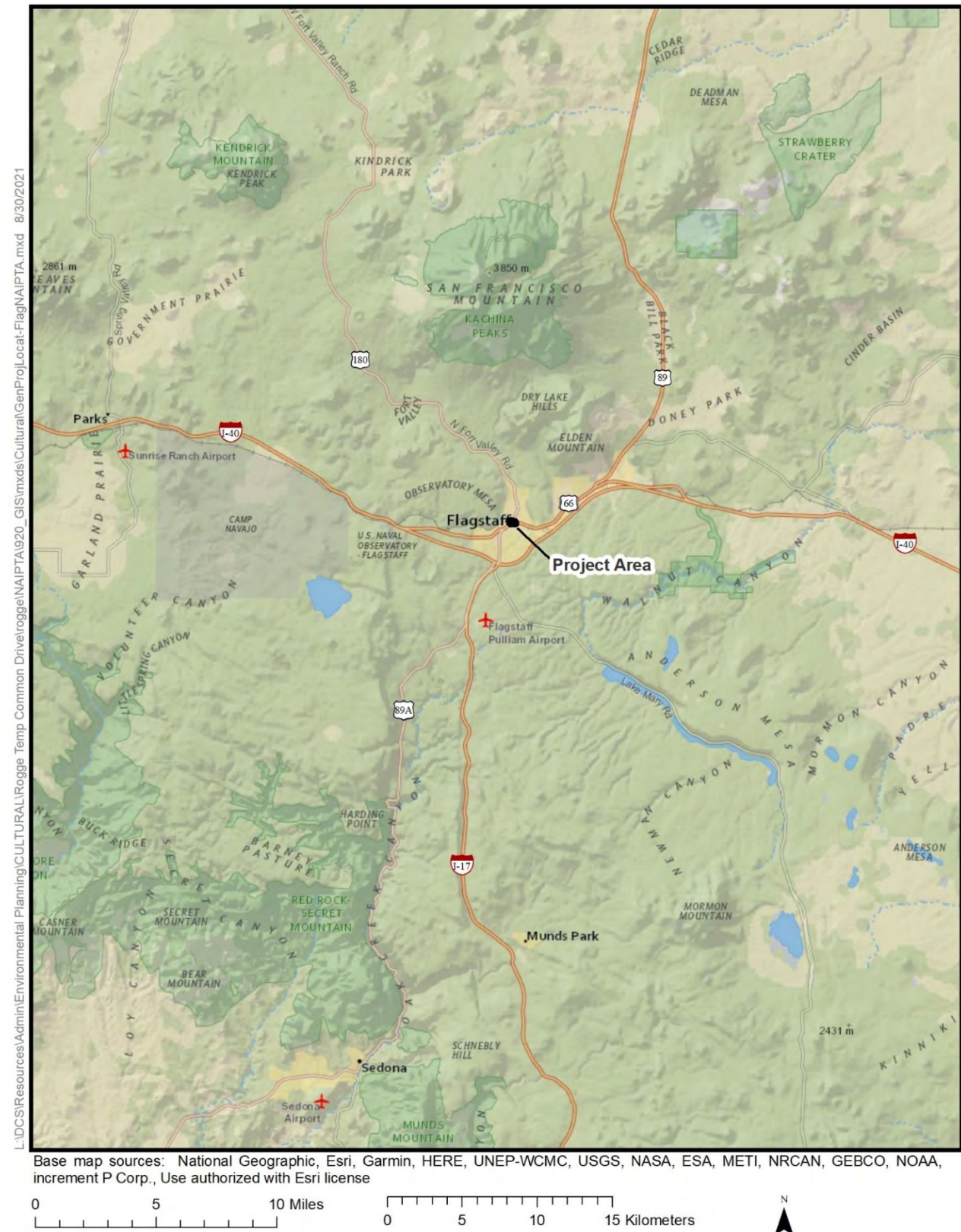


Figure 1. General Project Location

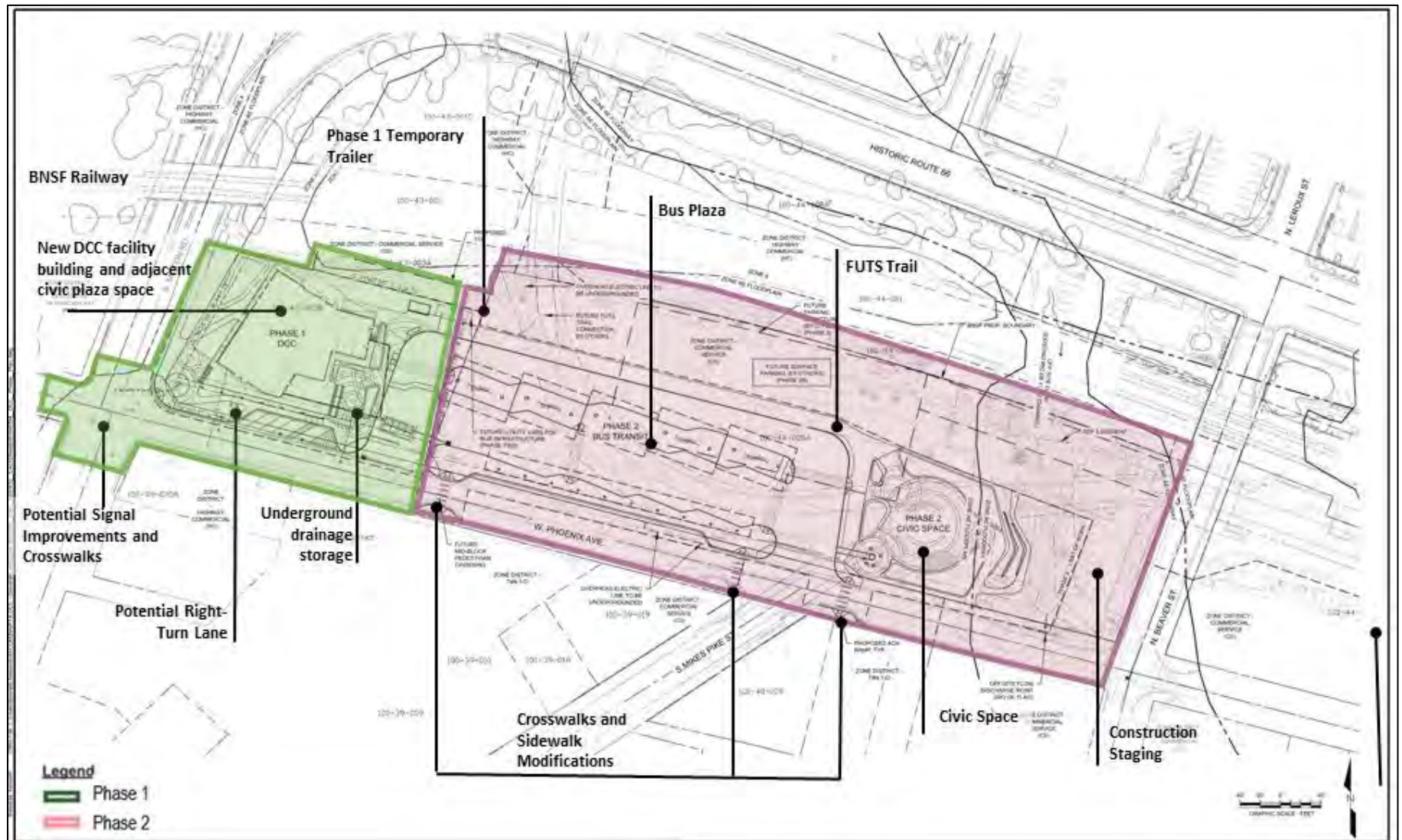


Figure 2. Site Plan and Construction Phasing

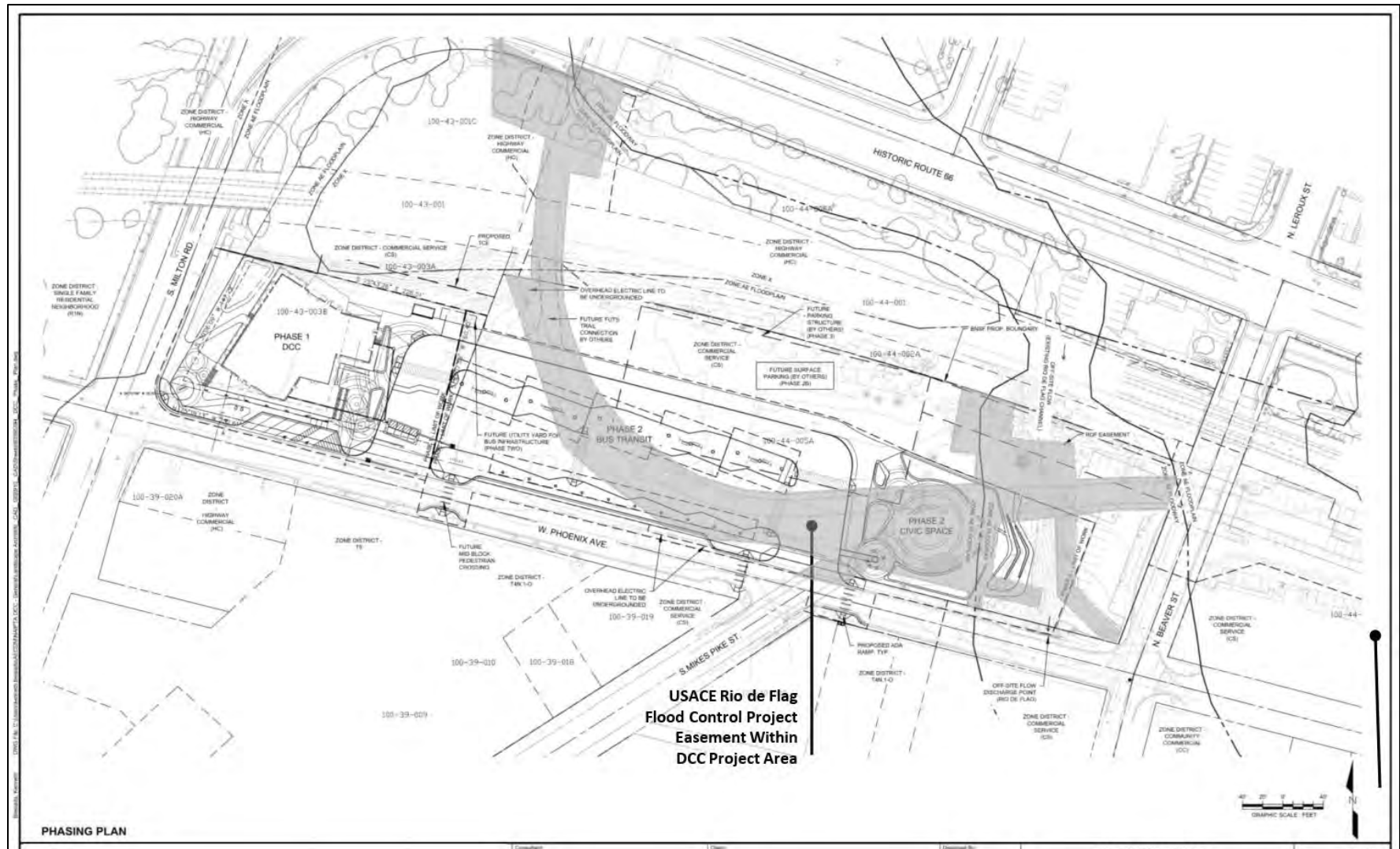


Figure 3. Site Plan and Rio de Flag Easement

- Installation of a temporary trailer with utility connections during Phase 1 construction to provide driver amenity space.
- Installation of electrical infrastructure to accommodate future Phase 2 bus electrical charging.

Phase 2 is located on the central and eastern sections of the site and overlaps with the Rio de Flag flood control project limits. It is anticipated Phase 2 would be constructed after the USACE Rio de Flag flood control project; construction is anticipated to be completed in 2025. Phase 2 includes:

- Construction of a bus plaza with 13 bus bays in two aisles including bus shelters or canopies, pedestrian walkways and crossings providing access to the bus bays, landscaping and shade trees, and space for installation of future amenities and infrastructure to service zero-emission bus technology such as electrical utilities and recharging stations.
- Construction of on-street improvements adjacent to the bus plaza along W. Phoenix Avenue, including a pick-up/drop-off area and loading zone.
- Construction of a civic space on the eastern section of the site, which is anticipated to include pedestrian walkways, seating, shade structures, overhead lighting, and landscaping.
- Construction of a trail along the northern section of the Project area as a component of a planned alignment of the Flagstaff Urban Trail System.
- Installation of pedestrian crossings on W. Phoenix Avenue, which would include Americans with Disabilities Act curb improvements and crosswalk striping at a new midblock crossing and at Mikes Pike.
- Installation of bicycle parking.

Additional roadway improvements in the Project area also are being considered, which may be constructed between Phase 1 and Phase 2. These improvements include installation of signals and a pedestrian crossing at the S. Milton Road/W. Phoenix Avenue intersection and construction of a right-turn lane from westbound Phoenix Avenue to northbound Milton Road. The Project would not require the construction of any off-site maintenance facilities, and it is anticipated that construction staging and storage areas and temporary relocation of the transit hub during construction would be confined to the Project area. Construction traffic is projected to add only a minor increment to the existing traffic volumes on adjacent roads.

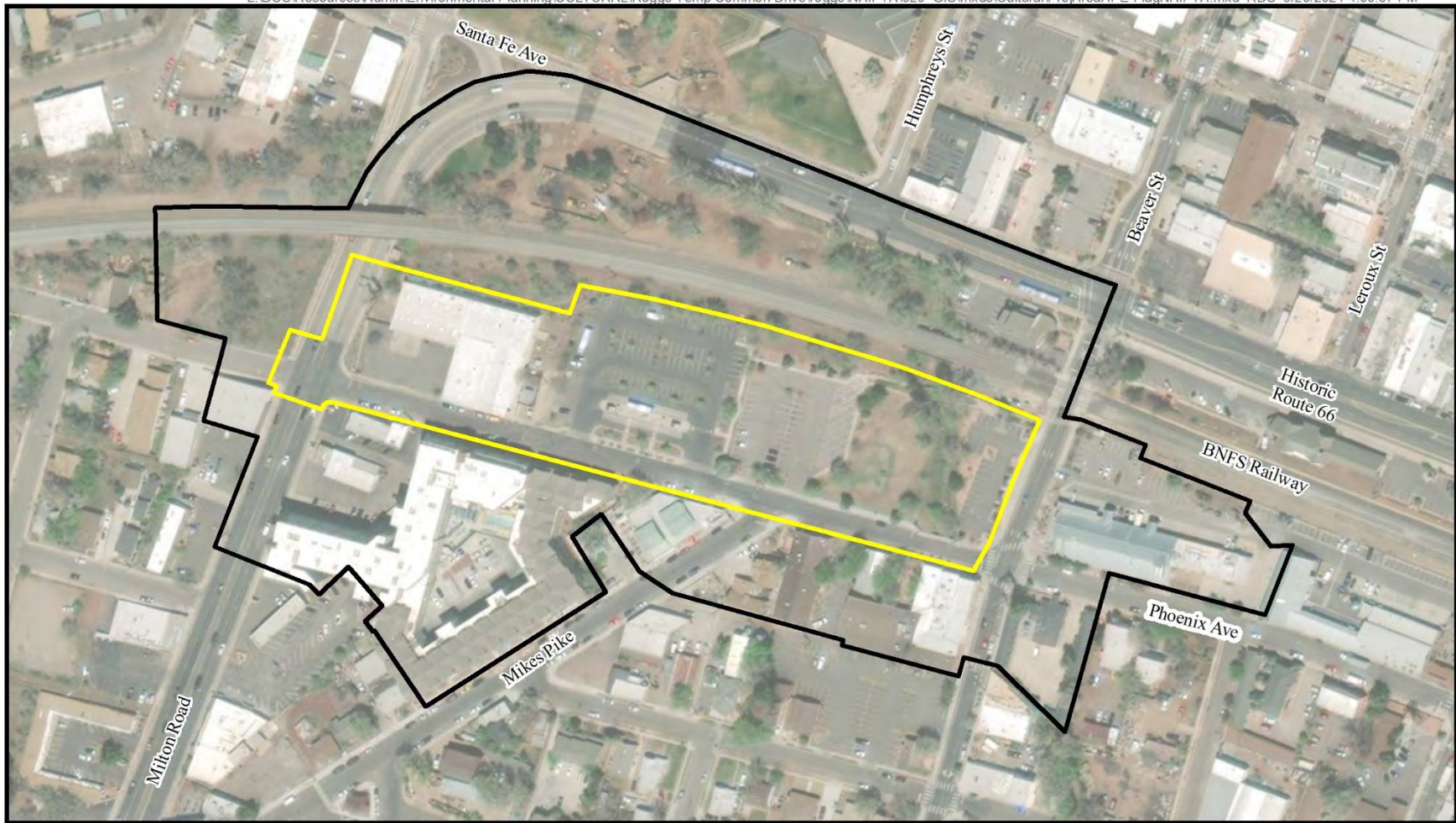
Project Location and Land Ownership

The Project is at 216 W. Phoenix Avenue, southwest of downtown Flagstaff. The Project area covers 5.8 acres bounded by the BNSF Railway to the north, W. Phoenix Avenue to the south, S. Beaver Street to the east, and S. Milton Road to the west (**Figure 4**). (The Project Area encompasses all elements of the ongoing concept planning as shown on Figures 2 and 3.) The Project area is in the SE1/4 of Section 16, Township 21 North, Range 7 East, Gila and Salt River Meridian, as mapped on the Flagstaff West, Arizona, US Geological Survey 7.5-minute topographic quadrangle (**Figure 5**). Most of the land in the Project area is owned by the City of Flagstaff but includes a small parcel owned by the BNSF Railway, which the City of Flagstaff is working to acquire. The existing DCC encompasses approximately 0.5 acre within the Project area. In addition to the current bus facility, the Project area is occupied by parking lots, a commercial building on the west end of the site, and the channel of the Rio de Flag. The Project is in an urban setting surrounded by commercial, residential, and transportation land uses.

Regulatory Requirements



Because FTA will be providing financial assistance for development of the DCC, the Project is a federal undertaking. Accordingly, potential impacts on the cultural environment are being assessed through a planning process compliant with Section 101(b)(4) of NEPA (42 USC § 4321 et seq.). Potential effects on properties listed in or eligible for the National Register of Historic Places (NRHP) are being considered concurrently, pursuant to Section 106 of the NHPA of 1966, as amended (54 USC § 306108) and its implementing regulations (36 CFR 800).

L:\DCS\Resources\Admin\Environmental Planning\CULTURAL\Rogge Temp Common Drive\rogge\NAIPTA\920 GIS\mxds\Cultural\ProjAreaAPE-FlagNAIPTA.mxd RDS 9/20/2021 4:00:57 PM



Aerial Source: Esri, Maxar, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

Legend

-  Project Area
-  Area of Potential Effects

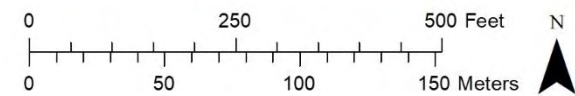


Figure 4. Project Area and Area of Potential Effects

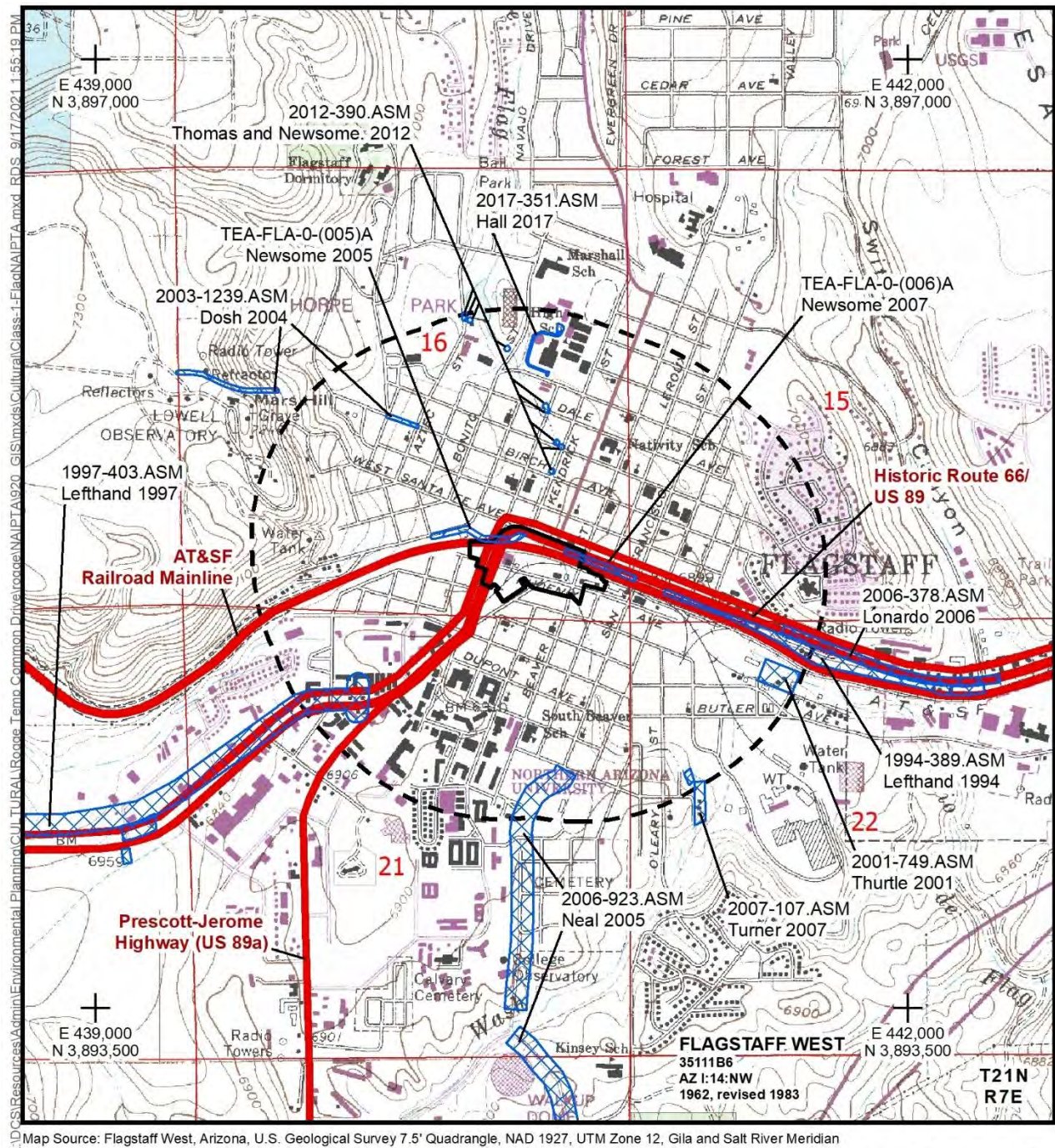


Figure 5. Prior Studies and Previously Recorded Historic In-Use Structures

In order for a historic resource to be considered eligible for listing in the NRHP, the resource must be evaluated by applying the NRHP criteria of eligibility pursuant to 36 CFR Part 60.4 (a-d), which states:

“...the quality of significance in American history, architecture, archeology, engineering and culture is present in districts, sites, buildings, structures and objects that possess integrity of location, design, setting, materials, workmanship, feeling and association and

- a) that are associated with events that have made a significant contribution to the broad patterns of our history; or
- b) that are associated with the lives of persons significant in our past; or
- c) that embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction; or
- d) that have yielded or may be likely to yield, information important in prehistory or history.”

A resource also must retain one or more aspects of integrity, including integrity of location, design, setting, materials, workmanship, feeling, or association. Overall, a resource must retain the defining features and characteristics that were present during its period of significance. However, the aspects of integrity that a resource must retain vary between the different kinds of resources.

Certain types of resources are generally not considered for listing in the NRHP, including religious properties, birthplaces and graves, cemeteries, reconstructed properties, commemorative properties, and resources achieving significance within the past 50 years. However, a resource that falls within one of those categories can be eligible for listing in the NRHP if it meets one of the following Criteria Considerations in conjunction with one or more of the four standard NRHP criteria listed above.

- a. a religious property that derives its primary significance from its distinctive art or architecture, or is historically important;
- b. a moved property that is primarily significant for architectural value or it is the only extant property associated with an important historic person or event;
- c. a birthplace or grave site of a historical figure if the person is of transcendent importance, and if it is the only extant property directly associated with the person's significance;
- d. a cemetery that is primarily significant because it contains graves of transcendent importance, from its age, its design, or association with historic events;
- e. a reconstructed property that is in a suitable environment and presented in a proper physical context and with a suitable interpretation in a master plan, and when it is the only surviving example of a property with the same associations;
- f. a commemorative property that has in itself gained significance in design, age, symbolic value, or tradition; and
- g. a property less than fifty years of age that is of exceptional importance.

Potential uses of historic resources also were considered in accordance with Section 4(f) of the Department of Transportation Act of 1966 (49 USC § 303). The intent of the statute is to avoid use or impairment of significant historic sites (or public park, recreation area, or wildlife refuge) for transportation projects or, where avoidance is not feasible and prudent, to minimize the use of such properties. Unless the use of a Section 4(f) property is determined to have a minor (*de minimis*) impact, FTA must determine that no feasible or prudent alternative exists before approving the use of such land for the Project. Feasible and prudent avoidance alternatives are those that avoid using any Section 4(f) property and do not cause other severe problems of a magnitude that substantially outweigh the importance of protecting the Section 4(f) property (23 CFR 744.17).

The cultural resource studies also considered requirements of the Arizona Antiquities Act (Arizona Revised Statutes §§ 41-841 through 41-844). That law prohibits collection of archaeological or vertebrate paleontological specimens and excavation of any historic or prehistoric ruin, burial ground, archaeological or vertebrate paleontological site or site including fossilized footprints, inscriptions made by human

agency or any other archaeological, paleontological or historical feature on lands owned or controlled by the State of Arizona or local governments without a permit issued by the Arizona State Museum (ASM). The act directs those in charge of activities on such lands to notify ASM of the discovery of any sites or objects that are at least 50 years old.

Because of the involvement of the Arizona Department of Transportation (ADOT), the cultural resource studies also addressed the State Historic Preservation Act (Arizona Revised Statutes §§ 41861 et seq.), which directs state agencies to provide the Arizona State Historic Preservation Office (SHPO) an opportunity to review and comment on state agency plans that could affect properties included in or eligible for the Arizona Register of Historic Places (ARHP). Criteria for listing in the ARHP are identical to those for the NRHP. potential impacts to historic properties.

The City of Flagstaff is a certified local government (CLG) under the State Historic Preservation Program. The City enacted a historic preservation ordinance (Flagstaff Zoning Code, Division 10-30.30), which established policy to preserve, protect, enhance, rehabilitate, and perpetuate historic properties, structures, sites, landmarks, and historic districts. The ordinance is implemented by designating landmarks and historic overlay zones, and requiring impacts of development on cultural resources be considered. The cultural resource studies addressed those requirements.

Area of Potential Effects

Regulations for Protection of Historic Properties, which implement NHPA Section 106 define the area of potential effects (APE) as “the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties” (36 CFR 800.16[d]). Delineation of the APE for both Phases 1 and 2 considered the nature, scale, and location of the DCC project and the potential of the project to affect historic properties directly and indirectly. In accordance with guidance of the Advisory Council on Historic Preservation (2019), direct effects were considered to occur at the time and place of project implementation with no intervening cause, and indirect effects to occur at a later time and further removed in distance but still reasonably foreseeable. Construction activities within the Project are have the most potential to physically disturb or destroy historic properties (including archaeological resources), but visual or noise impacts could also diminish the integrity of the setting of nearby historic properties and changes in access could also impact their use.

Because the City of Flagstaff is a CLG we coordinated with Mark Reavis, the Flagstaff Heritage Preservation Officer, in delineating the APE. The APE was delineated to include the Project area where the ground could be disturbed by construction activities, including construction staging areas, and encompasses approximately 5.8 acres between W. Phoenix Avenue on the south, the BNSF Railway right-of-way on the north, S. Milton Road to the west, and the west side of S. Beaver Street to the east. Ongoing design indicates the depth of disturbance resulting from construction of the new building, bus shelters, and other facilities; installation or relocation of buried utilities; and potential widening of W. Phoenix Avenue would be approximately 1.5 and 6 feet deep in the vicinity of the new building and underground stormwater storage tanks, and approximately 2 feet throughout the remainder of the site.

The APE was delineated to also include adjacent parcels of land, as defined by the Coconino County Assessor, adjacent to the Project area. Only part of a long parcel owned by the BNSF Railway extending along the railroad tracks far to the west of the Project area was included. The delineation resulted in an irregularly shaped above APE, but that seems appropriate because property parcels would typically serve as boundaries for any historic properties that might be affected. The APE covers 20.9 acres (see Figure 4). Although the Project would include a two story building, sight lines toward the Project diminish rapidly beyond the first tier of adjacent parcels because of the built environment and existing landscaping. No potential for visual and auditory impacts or changes in access were identified beyond the delineated APE. The assessment identified no potential for indirect effects that might occur at a later time and at greater distances.

Project Team

Kirsten Johnson, MA, was the principal investigator for the historic built environment component of the study. She is an architectural historian and meets the Secretary of the Interior's professional qualification standards for history and architectural history. A.E. (Gene) Rogge, PhD, was the principal investigator for the archaeological component of the study. He is a registered archaeologist and meets the Secretary of the Interior's professional standards for archaeology. Kirsten conducted a historic built environment survey of the APE on 12 August 2021, devoting one person-day to the fieldwork. No permit was required to conduct the survey of historic built environment resources.

Environmental and Cultural Contexts

Environmental Setting

The Project Area is approximately 6,900 feet above sea level at the southern edge of the San Francisco Volcanic Field of the Little Colorado Plateau physiographic province. The Project area is situated on a basalt lava flow from the Dry Lake volcano, which erupted during the Quaternary period approximately 0.8 to 1.6 million years ago about 4 miles southwest of what is now downtown Flagstaff (Holm 2019). The Natural Resource Conservation Service (2021) classifies soil in the western part of the Project area as Brolliar stony clay loam. Typically, about 15 percent of the ground surface of that soil unit is covered with cobbles, stones, or boulders, and the upper 3 inches of the typical soil column is stony clay loam, underlain by clay down to bedrock at a depth of approximately 30 to 40 inches. Soil in the eastern part of the project area along the course of the Rio de Flag is classified as Lynx loam to a depth of at least 5 feet.

Today, the Rio de Flag crosses the eastern part of the Project area but the historic channel alignment was approximately two blocks (700 to 800 feet) to the northeast (US Army Corps of Engineers 2000) (**Figure 6**). The Rio de Flag drains eastward to join Walnut Creek to form San Francisco Wash that flows into Padre Canyon and then Canyon Diablo before joining the Little Colorado River near the town of Leupp approximately 35 miles east of Flagstaff. Intermittent flows of the Rio de Flag would have provided an intermittent source of surface water in response to snow melt and precipitation events.

The Project is at an elevation of approximately 6,900 feet. The local climate has cold winters and moderate summers. Average minimum and maximum winter temperatures in December and January are 10 and 43 Fahrenheit and average summer temperatures in July are 45 and 81 degrees (Western Regional Climate Center 2021). The frost-free growing season is short—90 to 115 days (Natural Resource Conservation Service (2021)—which is about what is required for maize to mature.

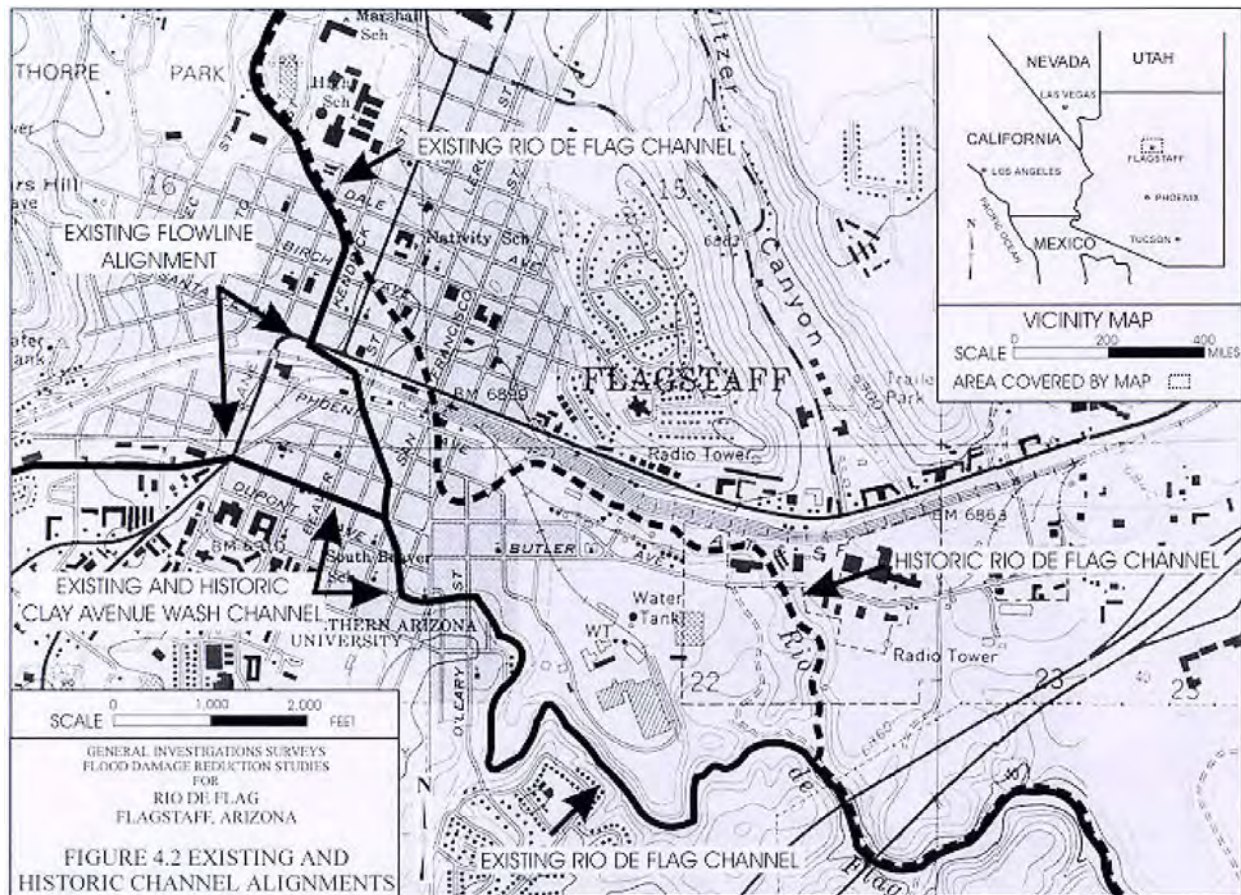
Natural vegetation in the Project Area would have been Petrean Montane Conifer Forest (Brown 1994), which is dominated by Ponderosa pine, and other trees that vary with elevation, including white pine, Douglas fir, white fir, quaking aspen, and Gambel oak. The understory includes several species of forbs and grasses. Fauna species common in that environment include large mammals such as mule deer, white-tailed deer, elk, gray wolf, and black bear, and many smaller mammal species such as porcupine, skunk, and several rodent species, along with many raptors, song birds, reptiles, and amphibians.

Cultural History Summary

The cultural history of north-central Arizona spans many millennia of human occupation. The prehistoric era can be divided into the Paleoindian, Archaic, Early Agricultural, Formative, and Late Prehistoric periods (Geib and Warburton 1991). The prehistoric era is followed by the temporally overlapping aboriginal Ethnohistoric period and the Historic period of Euro-American dominance (with African American and Asian Americans minorities as well).

Prehistoric Period

The earliest traces of humans in the region date to the late Pleistocene and early Holocene periods, perhaps as early as some 15,000 years ago (Cordell and McBrinn 2012; Goebel and others 2003; Haynes 2011; Mabry 1998; Politis and Prates 2019; Reid and Whittlesey 1997; Waters 2019). Archaeological sites of the first hunters and gatherers, referred to as Paleoindians, are rare but it is clear that those early populations pursued a nomadic lifeway that relied heavily on hunting of now extinct big-game species, such as mammoths, ancient bison, camels, and horses, but undoubtedly was supplemented with collected wild plant foods. Evidence of the Paleoindian era in the Project vicinity is limited to isolated finds of diagnostic spear points, which have been found at Wupatki, Winona, and Sedona (Anderson 1990:7-59).



Source: US Army Corps of Engineers 2000

Figure 6. Historic and Current Alignments of Rio de Flag in the Project Vicinity

The subsequent Archaic era [circa 7000 to 1000 Before the Current Era (BCE)] is a long, post-Pleistocene Period following the retreat of continental glaciers and the extinction of the large Pleistocene species. The Archaic era represents a continuation of a hunting and gathering lifeway adapted to a warmer and drier climate. Point types and other aspects of material culture differed markedly from those of the preceding Paleoindian Period. The dart and atlatl were used to hunt the smaller, swifter, and more elusive game, and gathering indigenous food plants took on greater importance. There is little evidence to suggest continuity between Paleoindian and Archaic populations on the Colorado Plateau (Geib and Warburton 1991).

The Archaic traditions of northern Arizona are classified as the Oshara in the east or Amargosa in the west. The Archaic era is divided into Early, Middle, and Late periods, based on changing styles of artifacts. Populations might have declined substantially during a period of extreme aridity between circa 4000 and 3000 BCE during the Middle Archaic period. The subsequent Late Archaic period has been characterized as a time of population increase, apparently coinciding with the advent of more favorable environmental conditions (for example, Petersen 1981) and the expansion of piñon pine, an important indigenous food (Berry and Berry 1986; Bungart 1990). The Late Archaic period began around 1700 BCE and ended with the introduction of cultigens, particularly maize, around 1000 to 600 BCE.

Recent chronometric data indicate that cultivation of maize during the White Dog phase of the early Basketmaker II period may have begun as early as 2000 BCE (Smiley 2002). Although some populations began the significant transition from complete reliance on hunting game and gathering of indigenous plant foods to growing cultigens, other populations did not and continued to pursue an Archaic period adaptation.

The Formative period, dated from about 400 to 1300 CE, is a stage of cultural development characterized by substantial reliance on agriculture, permanent or semi-permanent habitations, and the production of pottery (Minnis 1989). That shift, referred to as the Neolithic Revolution, or Neolithic Demographic Transition, reflects one of the most significant events in global cultural history as it transformed societies around the world (Bocquet-Appel 2011; Childe 1936). The Formative-era culture in the project area is labelled Sinagua and classified as an aspect of the Mogollon culture, but exhibits Ancestral Pueblo and Hohokam traits as well. Sinagua sites dominate the prehistoric component of the archaeological record in the Project vicinity.

Sinagua populations witnessed and were greatly affected by eruption of the volcano known as Sunset Crater, which was active for a few months to a few years between 1085 and 1090 Current Era (CE) (Elson 2011). Colton (1960) argued that the eruption stimulated a “land rush” because the volcanic ash and cinders that covered an extensive area surrounding the crater greatly enhanced agricultural potential by increasing soil water retention. More recent investigations concluded that cinders spewed from the volcano led to the evacuation of an estimated one to two thousand residents from more than 100 square miles because of the depth of deposited cinders, but did indeed create a nutrient rich mulch in the surrounding area where the cinders were less than 6 inches deep (Elson 2011).

Three pre-eruptive Sinagua phases have been defined—Cinder Park, Sunset, and Rio de Flag (Colton 1960; McGregor 1936, 1937; Pilles 1979). Colton originally defined the post-eruptive sequence as including three contemporaneous foci (Winona, Angell, and Padre) and the subsequent Elden Phase. Wilcox (1986) argued that the Winona, Angell, and Padre foci were instead consecutive temporal phases, and defined two post-Elden phases—Turkey Hill (1200 to 1300 CE) and Clear Creek (1300 to 1400 CE). Pilles and Stein (1981) argued that when the Sinagua abandoned their homeland, some populations moved south into the middle Verde River region and others amalgamated with other groups to form the ethnohistoric Hopi.

Ethnohistoric and Historic Periods

Several tribes probably used the area around Flagstaff during the ethnohistoric era, including the Navajo and Tonto Apache, as well as the Hopi, Havasupai, and Hualapai who were trading partners, and perhaps the Yavapai and Southern Paiute (Spicer 1962).

Although Spanish explorers began traveling through what is now Arizona in the sixteenth century, Spanish colonization of Arizona never expanded north of the Tucson area except for a 1629-1680 mission among the Hopi villages (Spicer 1962). Nevertheless, the introduction of European diseases and domesticated crops and animals, as well as the creation of a market for slaves, had a profound effect on the indigenous tribal cultures. After the United States acquired the region at the conclusion of the 1846–1848 War with Mexico, the pace of Euro-American settlement quickened dramatically, and completely transformed the region within half a century.

In the 1850s, the US Congress authorized five surveys between Texas and California in an effort to identify a safe and efficient travel and communication route between the East and West coasts (Newsome 2007). In 1851, Captain Lorenzo Sitgreaves traveled through the Flagstaff region while exploring possible wagon road and railroad routes along the 35th parallel, and Lieutenant Amiel W. Whipple followed in 1853 and 1854 (Dodge 1980:67; Hall 1906:476; Morris and others 1994). Lieutenant Edward F. Beale built a wagon road along this corridor in the late 1850s (Morris and others 1994).

In May 1876, a group of 45 emigrants from Boston organized by the Arizona Colonization Company briefly settled an area approximately six or seven miles north of the Flagstaff's current location and laid out a townsite known as Agassiz. The settlers soon became discouraged by the poor prospects for farming and mining, and after second group arrived in July 1876, both parties relocated to Prescott and many have eventually traveled to California before returning to Boston. Prior to leaving the area, the emigrants reportedly celebrated Independence Day by stripping a lone pine tree near the intersection of Fort Valley Road and the Overland Route and raised an American flag. The 1878 General Land Office (GLO) plat identified this location as “Flagstaff” (Woodward and others 1985a).

The town of Flagstaff was established in the 1880s in anticipation of the arrival of the Atlantic & Pacific Railroad (A&P).¹ An act of the 39th Congress granted the A&P alternate, odd-numbered sections of land for 40 miles on either side of the line, which closely followed the wagon road previously built by Lieutenant Beale. In the summer of 1880, the A&P surveyors and grading crew established a construction terminus camp in present-day Flagstaff at Antelope Spring near the base of Mars Hill. Although this location was convenient to water and near the crossroads of the Overland Route and the Fort Valley Road, the A&P did not intend to develop it as a railroad townsite. In 1881, however, the construction terminus camp became a boom town, beginning with the establishment of a store by Prescott merchant, P.J. Brannen, who was soon followed by other merchants who opened saloons and other businesses north of the railroad's right-of-way. This area became Flagstaff's first commercial center and eventually came to be known as "Old Town" (Woodward and others 1985a).

Construction of the A&P revolutionized the lumber industry due to the high demand for railroad ties and lumber for railroad bridges. In 1881, Midwestern entrepreneur and lumberman, Edward E. Ayer, arrived in Flagstaff and secured contracts to supply lumber to the A&P. The Ayer Lumber Company opened in August 1882, two weeks before the A&P reached Flagstaff. The mill was located southwest of the intersection of present-day S. Milton Road and Historic Route 66 (Stein 1993). Mr. Ayer hired Denis Matthew "Matt" Riordan to manage the company. Between 1886 and 1877, Mr. Riordan purchased the company from his employer and reorganized the business as the Arizona Lumber Company. Shortly after this purchase, Riordan, along with his business associate, Francis Hinckley, acquired the Arizona Mineral Belt Railroad to deliver lumber to the mill and export lumber to market, establishing the first logging railroad in Flagstaff area. The Arizona Mineral Belt Railroad had been incorporated by Colonel James W. Eddy as a spur line in 1883 to transport minerals and lumber between Flagstaff and the mining town of Globe. The A&P, Dr. P.J. Brannen, and Mr. Riordan, invested in the spur line, but by 1887, only 35 miles of line had been built due to financial difficulties arising from the A&P's refusal to fulfill its agreement to reimburse Eddy for construction costs. Riordan reincorporated the railroad as the Central Arizona Railway Company (CARY). By the end of 1889, Riordan's company had negotiated 25-year contracts for almost all the timber owned in the Flagstaff vicinity by the A&P and two cattle companies, and at the beginning of the following year, the company was reincorporated as the Arizona Lumber and Timber Company (Newsome 2007; Stein 1993; Trennert 1970; Woodward and others 1985a).

When the A&P was completed to Flagstaff in August 1882, a depot was constructed one-half mile east of the Old Town settlement in flatter terrain. Seeing the advantages of proximity to the depot, Brannen and many other Old Town merchants relocated to the area opposite the depot, which came to be known as "New Town." After a fire destroyed the row of commercial buildings remaining in Old Town in 1884, New Town was solidified as the new commercial center of Flagstaff. Commercial properties lined Railroad Avenue (today's Historic Route 66) opposite the depot, and residential properties were constructed on Leroux Street between the commercial area and Cherry Street (Woodward and other 1985a).

The plat for the Flagstaff Townsite was patented in January 1890, at which time Flagstaff already boasted two banks. In 1890, a private electric light plant was built and a telephone system was installed, and a fire department was organized during that decade. In 1891, the Territorial Legislative Assembly passed an act creating Coconino County, and Flagstaff was voted county seat by a special election. In 1894, construction of the County Courthouse was completed, the Town of Flagstaff incorporated, and the Lowell Observatory was constructed on Mars Hill. These events, along with the recording of two additional subdivisions – the Railroad Addition and Brannen Addition – resulted in a building boom in the late 1890s. The Railroad Addition extended the townsite east from Leroux Street and the Brannen Addition was south of the railroad right-of-way in the NE1/4 of Section 21, south of the railroad. In 1899, the Territorial Legislature established the Arizona Normal School in Flagstaff. By 1900, the population of Flagstaff was just under 2,000 people and the town had become a major rail center with four lumber mills. The local economy also was supported by sheep and cattle ranching (Newsome 2007; Stein 1993; Woodward and others 1985a, 1993).

¹ In 1881, the A&P became a subsidiary of the Atchison, Topeka and Santa Fe Railway (AT&SF), but continued to operate under the A&P name until the late nineteenth century.

Between 1894 and 1935, Old Town became more residential in character as the commercial center grew at the intersection of Leroux and Railroad avenues within the Railroad Addition. Residences and churches were constructed in the Brannen Addition and north of the business district within the Railroad Addition. Commercial businesses also were constructed south of the railroad along Phoenix Avenue and Beaver Street, which mainly provided services to rail travelers, and warehouses were built along the railroad right-of-way between Leroux and San Francisco streets. Additional subdivisions were platted in 1916, 1919, and 1924 in response to the continuing growth and prosperity of the community (Newsome 2007; Stein 1993; Woodward and others 1993).

As the popularity of automobile travel increased in the early twentieth century, Flagstaff leaders began to promote the Beale Road as a major automobile route. In the early 1910s, the Santa Fe-Grand Canyon-Needles National Highway Association, which was affiliated with the National Old Trails Association, appealed to Congress to route a federally funded coast-to-coast highway along the Beale Road. In 1913, a highway route through Winslow, Flagstaff, and Williams was officially designated as the National Old Trails Road. In the 1920s, the roadway was improved and redesignated as US Route 66. US Highway 89, a north-south route between Fredonia on the Utah-Arizona border and the US-Mexican border, also was constructed in the 1920s and followed the same alignment as Route 66 through Flagstaff (Newsome 2007).

Tourism bolstered the local economy in Flagstaff and throughout Arizona in the 1920s. The increased auto traffic through Flagstaff resulted in the construction of new roadside businesses, such as gas stations, diners, and motels. Between 1920 and 1930, the population of Flagstaff increased by approximately 18 percent (Southard 2013). Flagstaff incorporated as a city in 1928, and in 1929, Flagstaff's first motel, the Motel Du Beau, was constructed adjacent to Route 66 southeast of the intersection of S. Beaver Street and W. Phoenix Avenue (City of Flagstaff 2021; Paradis 2003).² In 1930, the planet Pluto was discovered from the Lowell Observatory (Olberding 2014a).

Flagstaff's economy declined during the first half of the 1930s with the onset of the Great Depression and was further stressed by the multitude of unemployed migrants traveling on Route 66 to California. However, due to the importance of Route 66 to cross-country travel, Arizona received federal funding under the New Deal programs for road improvements, which stimulated the Flagstaff's economy. The increased use of Route 66 during the Depression era resulted in substantial traffic bottle necks at the Atchison, Topeka & Santa Fe Railway (AT&SF) crossing at S. Beaver Street. To alleviate this problem, the City of Flagstaff applied for and received funding for a new underpass at the intersection of the AT&SF and present-day S. Milton Road, which was constructed as part of the realignment and improvement of Route 66 financed by previously allocated New Deal funding. The installation of the underpass re-routed Route 66 to its current alignment through Flagstaff, and the section of the original alignment along S. Beaver Street, W. Phoenix Avenue, and Mikes Pike is no longer considered part of the highway. Route 66 was fully paved in 1938 (Hardy 2018; Paradis 2003; Southard 2013).

During World War II, the newly improved Route 66 was used to transport troops and supplies across the US, and the Navajo Ordnance Depot was established 12 miles west of Flagstaff in Bellemont. The arrival of soldiers and their families doubled Flagstaff's population (Olberding 2014a). In the 1950s, the Flagstaff Housing Authority completed an urban renewal project to improve the quality of housing in the Southside Neighborhood, which was primarily inhabited by individuals of Spanish/Basque, Mexican, and Mexican-American Heritage (Arizona Archives Online 2016). In 1955, the U.S. Naval Observatory was established near Flagstaff for dark-sky defense monitoring. Between 1959 and 1962, the Glen Canyon Dam was constructed north of Flagstaff, which resulted in the construction of an improved road between Flagstaff and the Verde Valley that would eventually be designated as Interstate 17 and provide a direct connection between Flagstaff and Phoenix. Flagstaff continued to prosper in the 1960s (Olberding 2014a).

In the 1970s and 1980s, downtown Flagstaff experienced a decline. The completion of Interstate 40 diverted traffic from the Route 66 alignment, and downtown infrastructure was not expanded to

² Between 1926 and 1934, US Route 66 through Flagstaff followed the alignment of the National Old Trails Road, which branched south from the current alignment at S. Beaver Street, continued west on W. Phoenix Avenue, and then turned southwest and followed Mikes Pike until its intersection with S. Milton Road (Paradis 2003).

accommodate the growing residential and student population of Northern Arizona University. Many downtown historic buildings were demolished. Retail businesses moved out of downtown and relocated to the new Flagstaff Mall (Olberding 2014b). In the 1990s, the Flagstaff's downtown began to be revitalized using funding provided by the Main Street program and through the construction of new government buildings.

Records Review

Review Methods

The study team reviewed records to identify and compile information about prior cultural resource studies and recorded cultural resources in the APE and a surrounding 0.5-mile buffer, which is consistent with SHPO guidelines for records reviews in urban areas. The reviews sources of information included

- NRHP listings,
- Arizona Register of Historic Places listings,
- records of the City of Flagstaff Heritage Preservation Office,
- AZSITE Cultural Resource Inventory geospatial database and associated project records and archaeological site forms,
- records of cultural resource studies and recorded resources on file at the ASM Archaeological Records Office but not incorporated into the AZSITE database, and
- historic maps [GLO plats, Sanborn Fire Insurance (Sanborn) maps], and
- historic aerial photos.

Prior Cultural Resource Surveys

The review identified 17 prior cultural resource surveys conducted between 1980 and 2017 within or overlapping the records review area (see Figure 4). Six of those covered parts of the APE. The other 11 covered small parts of the buffer around the APE (**Table 1**).

Table 1. Prior Cultural Resource Studies in the Records Review Area ⁽¹⁾

	Project Name and Number	Scope	Results	Reference
Studies within the APE				
1	Flagstaff Historic Property Survey*	Flagstaff city limits	1 historic district (Railroad Addition Historic District [original boundary]) and 6 NRHP individually-listed buildings in buffer	Janus Design 1980; Garrison and Woodward 1980
2	Flagstaff Historic Properties Survey/Flagstaff Multiple Resource Area*	Flagstaff city limits	1 historic district overlaps APE (Railroad Addition Historic District Expansion); 2 historic districts (Flagstaff Townsite Historic Residential District, North End Historic Residential District) and 14 individually listed buildings in buffer	Woodward and others 1985a, 1985b
3	City of Flagstaff Southside/Old Town historic building survey*	area bounded by Santa Fe Railroad tracks, Northern Arizona University campus, Lone Pine Rd., and Blackbird Roost Rd. alignment	2 historic districts overlap APE [Flagstaff Southside Historic District and Railroad Addition Historic District (expanded boundary)]	Woodward and others 1993; Michael Wilson Kelly Architects 2009
4	Flagstaff Urban Trail survey – West Village to Rio de Flag, ADOT TEA-FLA-0(005)A	2 acres	4 previously recorded historic structures in APE [collocated Historic Route 66/ AZ I:15:156(ASM), US 89/ AZ I:3:10(ASM), AT&SF Railroad Mainline/AZ I:14:334(ASM), and Flagstaff Underpass/ADOT 529]; and 3 previously recorded historic-period buildings in buffer (J.C. Milligan House, First Methodist Church, Carl Walters House)	Newsome 2005

Project Name and Number		Scope	Results	Reference
5	Flagstaff Urban Trail survey – Rio de Flag to San Francisco Street, ADOT TEA-FLA-0-(006)A	1.3 acres	3 previously recorded historic structures in APE [collocated Historic Route 66/ AZ I:15:156(ASM), US 89/ AZ I:3:10(ASM), AT&SF Railroad Mainline/ AZ I:14:334 (ASM)]; and 1 previously recorded historic-period building in buffer (Santa Fe Passenger Depot)	Newsome 2007
6	ADOT bridge inventory	statewide	2,504 structures built before 1964, 2 in APE (Phoenix Avenue Bridge/ADOT 8709 and Flagstaff Underpass/ADOT 529)	FRASERdesign 2008
7	Rio de Flag supplemental cultural resource survey*	undetermined	6 historic structures, 1 in APE; (Arizona Mineral Belt Trestle), 5 in buffer (bridge, railroad loading dock, 3 walls)	USACE 2021
Prior Studies within the 0.5-mile buffer area				
8	Historic Route 66 bike path survey, 1994-389.ASM	4.3 acres	no sites	Lefthand 1994
9	State Route 40B landscaping and irrigation survey, 1997-403.ASM	8.04 acres	1 previously recorded historic structure in APE [collocated Historic Route 66/ AZ I:15:156(ASM) and US 89/ AZ I:3:10(ASM)]	Lefthand 1997
10	Cemex plant site survey, 2001-749.ASM	2.84 acres	no sites	Thurtle 2001
11	Arizona Snowbowl proposed water pipeline survey, 2003-1239.ASM	9.87 acres	1 previously recorded historic property in buffer [Lowell Observatory National Historic Landmark, AZ I:14:33(ASM)]	Dosh 2004
12	State Route 40B survey (Pine Springs to Switzer Canyon), 2006-378.ASM	6.38 acres	1 previously recorded historic structure in APE [collocated Historic Route 66/ AZ I:15:156(ASM) and US 89/ AZ I:3:10(ASM)]	Lonardo 2006
13	Northern Arizona University South San Francisco Street sidewalk survey, 2006-923.ASM	3.1 acres	2 sites, neither in records review area	Neal 2005
14	Aspen Park survey, 2007-107.ASM	1.74 acres	no sites	Turner 2007
15	Flagstaff Urban Trail System sign installation survey, 2012-390.ASM	1 acre	3 previously recorded historic structures in APE [collocated Historic Route 66/ AZ I:15:156(ASM), US 89/ AZ I:3:10(ASM)], and Prescott-Jerome Highway/U.S. 89A/AZ N:7:61(ASM)	Thomas and Newsome 2012
16	telecommunications tower survey, 2017-23.ASM*	undetermined	no sites	Antigua Archaeology 2017
17	AZ10-069 Eagle/AZ2 Aperture telecommunications facility survey, 2017-351.ASM	less than 1 acre	no sites	Hall 2017

NOTES: (1) The review area included the APE and a surrounding buffer 0.5 mile wide

*not mapped in Figure 4

ADOT = Arizona Department of Transportation, NRHP = National Register of Historic Places, USACE = US Army Corps of Engineers

Three of the surveys that covered parts of the APE (as well as part of the surrounding 0.5-mile buffer) were historic-period building inventory surveys. The Flagstaff Historic Property Survey identified the Railroad Historic District and six individually significant buildings in the buffer, all of which were subsequently listed in the NRHP in 1983 (Janus Design 1980; Garrison and Woodward 1980). The original boundary of the Railroad Addition Historic District did not include properties south of Historic Route 66, but the subsequent Flagstaff Multiple Resource Area (MRA) survey expanded the boundary of the Railroad Addition Historic District to the south (Woodward and others and 1985a, 1985b), and the expansion overlaps the APE. The MRA survey also identified two additional historic residential districts (Flagstaff Townsite Residential District and North End Residential District) and 14 buildings that are in the buffer and all were subsequently listed in the NRHP. Woodward and others (1993) completed a survey of

the Southside/Old Town area, and defined the Flagstaff Southside Historic District, which overlaps the APE. That district was listed in the NRHP in 2010 (Michael Wilson Kelly Architects 2009).

One of the other prior studies was a statewide inventory of ADOT bridges. Both inventoried bridges are in the APE.

The 10 other prior studies were surveys for built environment and archaeological resources, but no archaeological resources were identified. All 10 of those surveys were of limited scope, each covering fewer than 10 acres, and in the aggregate, they covered at most, no more than a few percent of the review area.

Three of those 10 surveys covered parts of the APE. Two covered parts of the Flagstaff urban trail system and identified the previously recorded collocated Historic Route 66 and US Highway 89, the AT&SF Mainline, and the Flagstaff Underpass (ADOT 529) within the APE, and identified four previously recorded historic-period buildings in the buffer. The historic-period buildings include the J.C. Milligan House, which is individually listed in the NRHP and is a contributor to the NRHP-listed Flagstaff Townsite Historic District; the First Methodist Church and the Carl Walters House, which are listed as contributors to the Flagstaff Townsite Historic District; and the Santa Fe Passenger Depot, which has previously been evaluated as individually eligible and is listed as a contributor to the NRHP-listed Railroad Addition Historic District. The other survey covered parts of the Rio de Flag flood control project and recorded a trestle of the Arizona Mineral Belt Railroad in the APE, and 3 walls, a bridge, and a railroad loading dock in the buffer.

Previously Recorded Cultural Resources

The records review identified 18 historic buildings and structures within the APE, including two historic districts, 9 contributing properties within those districts, 3 highways, 3 bridges, and one railroad (**Table 2**; **Figure 7**; see Figure 4). The review also identified 3 NRHP-listed historic districts and 20 individually NRHP-listed buildings in the 0.5-mile buffer outside the APE (see Appendix A).

Table 2. Previously Recorded Historic Buildings and Structures in the APE

Property Name	Description	NRHP Status	Reference
Historic Districts			
1 Flagstaff Southside Historic District	commercial and residential district bounded by Mike's Pike and Beaver St. on the west , Franklin Ave. Ellery Ave., and Rio de Flag on the south, Elden St. and Lone Pine Rd. on the east, and Phoenix Ave. on the north	listed, Criteria A and C	Woodward and others 1993; Michael Wilson Kelly Architects 2009
2 Railroad Addition Historic District AZ I:15:53(ASM)	commercial district roughly bounded by Birch Ave. to the north, N. Leroux St. and S. Beaver St. to the west, the W. Phoenix Ave. alley to the south, and N. Agassiz St. and S. San Francisco St. to the east	listed, Criteria A and C	Garrison and Woodward 1980; Janus Design 1980; Woodward and others 1985a, 1985b; Woodward and others 1993
Individual Buildings and In-Use Historic Structures			
3 Arizona Central Commercial Co. Warehouse 2 (24) S. Beaver St.	Panel Brick Commercial-style building constructed with Moenkopi sandstone in 1924	listed as contributor to the Railroad Addition Historic District	Woodward and others 1985a, 1985b; Woodward and others 1993; Heng and Buckham 2021
4 Arizona Machinery Co. Building 7-9 S. Beaver St.	large vernacular industrial building constructed between 1934 and 1943; excellent illustration of malpais stone construction with steel bowstring truss roof system	listed as contributor to Flagstaff Southside Historic District	Woodward and others 1993; Michael Wilson Kelly Architects 2009; Heng and Buckham 2021
5 Atchison, Topeka & Santa Fe Railroad Mainline AZ I:14:334 (ASM)	historic railroad alignment constructed in 1882	determined eligible under Criteria A and D	Dosh 1996; Newsome 2005, 2007

Property Name	Description	NRHP Status	Reference
6 Coca-Cola Bottling Works 119 W. Phoenix Ave.	vernacular commercial building constructed of random Malpais stone between 1937 and 1942	listed as contributor to Flagstaff Southside Historic District	Woodward and others 1993; Michael Wilson Kelly Architects 2009; Heng and Buckham 2021
7 Du Beau Motel 19 W. Phoenix Ave.	Bungalow-style motel constructed in 1929; first motel constructed in Flagstaff	listed as contributor to the Railroad Addition Historic District	Woodward and others 1985a, 1985b; Woodward and others 1993; Heng and Buckham 2021
8 Flagstaff Steam Laundry 210 W. Phoenix Ave.	Panel Brick Commercial-style building constructed in 1925; cast-in-place concrete frame	listed as contributor to Flagstaff Southside Historic District	Woodward and others 1993; Michael Wilson Kelly Architects 2009; Heng and Buckham 2021
9 Flagstaff Underpass, ADOT 00529	2-span I-beam stringer bridge with steel baluster guardrails constructed in 1958	determined not eligible	Newsome 2005; FRASERdesign 2008
10 Historic Route 66 AZ I:15:156(ASM)	historic alignment of US Route 66; shares alignment with US 89, US 89A, S. Milton Rd., and State Business Route 50 within the APE	determined eligible, Criteria A and C	Weaver 1990; Newsome 2005, 2007; Thomas and Newsome 2012
11 Hutchison Building 111 (107) W. Phoenix Ave.	utilitarian commercial building constructed of local Malpais stone between 1937 and 1942	listed as contributor to Flagstaff Southside Historic District	Woodward and others 1993; Michael Wilson Kelly Architects 2009; Heng and Buckham 2021
12 Hutchison & Sauer Building 1-7 S. Beaver St.	Pueblo Revival-style building constructed between 1930 and 1935; exhibits cement-based plaster sheathing installed to simulate random ashlar stone	listed as contributor to Flagstaff Southside Historic District	Woodward and others 1993; Michael Wilson Kelly Architects 2009; Heng and Buckham 2021
13 Phoenix Avenue Bridge, ADOT 08709 W. Phoenix Avenue, west of Beaver St.	2-span concrete slab bridge with concrete guardrails with a blind balustrade constructed in 1929	determined not eligible	FRASERdesign 2008
14 Prescott-Jerome Highway/ US 89A AZ N:7:61(ASM)	in-use state highway; shares alignment with US 89, Historic Route 66, S. Milton Rd., and State Business Route 50 within the APE	segments determined eligible, Criterion D	Hathaway and Gregory 1991; Thomas and Newsome 2012
15 Marcos Café 113 W. Phoenix Ave.	utilitarian commercial building constructed of local Malpais stone between 1941 and 1943	listed as contributor to Flagstaff Southside Historic District	Woodward and others 1993; Michael Wilson Kelly Architects 2009; Heng and Buckham 2021
16 Arizona Mineral Belt Railroad Trestle	former railroad trestle repurposed as a pedestrian footbridge	determined eligible, Criterion not specified	USACE 2021
17 T.E. Pullium House 125 W. Phoenix Ave./12 S. Mike's Pike	Queen Anne Cottage constructed between 1894 and 1897	recommended eligible as a contributor to the Flagstaff Southside Historic District	Woodward and others 1993; Michael Wilson Kelly Architects 2009; Heng and Buckham 2021
18 US Highway 89 AZ I:3:10(ASM)	in-use state highway; shares alignment with US 89A, Historic Route 66, S. Milton Rd., and State Business Route 50 within the APE	segments determined eligible, Criterion D	Spaulding and Lefthand 1995; Newsome 2005, 2007

The Flagstaff Southside Historic District was listed in the NRHP in 2010 under Criteria A and C on the local level of significance (Woodward and others 1993; Michael Wilson Kelly Architects 2009). The district is roughly bounded by Mike's Pike and Beaver St. on the west, Franklin Ave. Ellery Ave., and Rio de Flag on the south, Elden St. and Lone Pine Rd. on the east, and Phoenix Ave. on the north. The period of significance is 1886 to 1947, and primarily includes residential properties with two well-defined commercial areas. The district is significant under Criterion A for associations with the growth and development of Flagstaff after 1900 and as the principal residential area of workers in the lumber and sheep industries. The district reflects Flagstaff's evolving ethnic diversity from the early twentieth century through the 1940s and includes Flagstaff's largest neighborhood with minority populations including African-Americans, Hispanics, and Basques. The district is significant under Criterion C for an array of vernacular residences and commercial buildings that represent building types once prevalent in Flagstaff. The district features many Bungalow-style residences and a concentration of Amerikanuak Vernacular buildings, which are local adaptations of traditional Basque homes.

L:\DCS\Resources\Admin\Environmental Planning\CULTURAL\Rogge Temp Common Drive\rogge\NAIPTA\920 GIS\mxds\Cultural\HistoricBuildings-FlagNAIPTA.mxd RDS 9/20/2021 12:27:09 PM

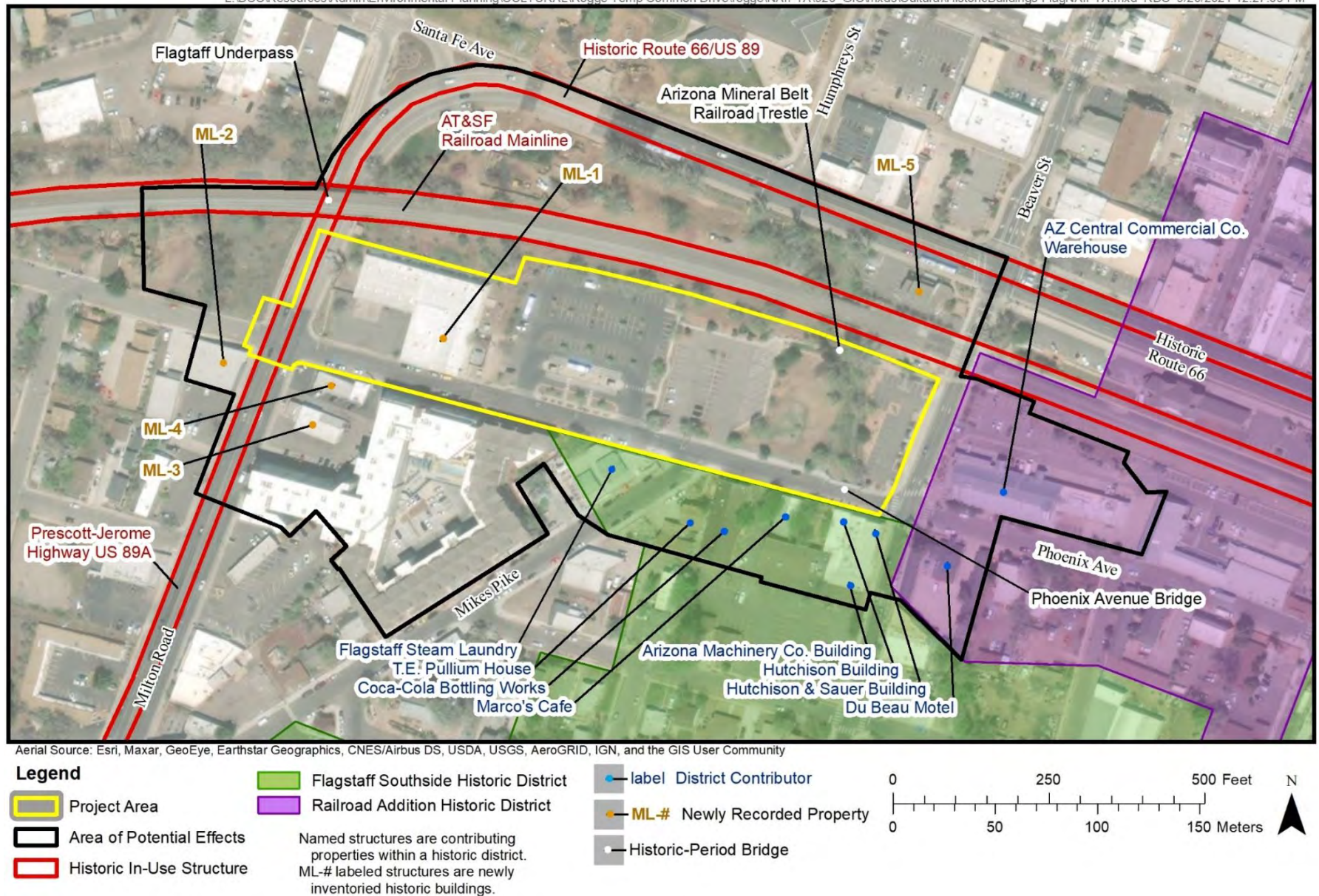


Figure 7. Historic-Period Properties in the APE

Of the 142 contributing properties within the district, seven are within the Project APE along W. Phoenix Avenue. Of the seven contributors within the APE, only the T.E. Pulliam House was constructed as a residential property. This Queen Anne-style cottage was built between 1894 and 1897 and currently operates as a restaurant. The Pulliam House is not listed as a contributing property on the Flagstaff Southside Historic District, but the Flagstaff Heritage Preservation Office is currently updating the Arizona Historic Property Inventory Forms for the properties within the Flagstaff Southside Historic District, and is recommending the T.E. Pulliam House be considered eligible as a contributor to the NRHP-listed district (Heng and Buckham 2021).

The other six contributors are within the W. Phoenix Avenue commercial area, which developed due to its proximity to the railroad alignment and its location along the Old National Trails Highway and Route 66 alignment until 1934. The Panel Brick-style Flagstaff Steam Laundry was the first of the contributors to be constructed in 1925. The other five commercial properties were built between 1930 and 1943. Three feature local malpais stone construction (Arizona Machinery Co. Building, Coca-Cola Bottling Works, Hutchison Building) and the Hutchison & Sauer Building exhibits cement-based plaster sheathing installed to simulate random ashlar stone (Michael Wilson Kelly Architects 2009; Woodward and others 1993).

The Railroad Addition Historic District is roughly bounded by Birch Avenue to the north, N. Leroux Street and S. Beaver Street to the west, the W. Phoenix Avenue alley to the south, and N. Agassiz Street and S. San Francisco Street to the east. The period of significance is 1882 to 1945. The district is significant under NRHP Criterion A as an embodiment of the pattern of Flagstaff's commercial development and community growth beginning with the arrival of the railroad in 1882 and continuing through the post-World War II expansion. The district also is significant under NRHP Criterion C as a collection of buildings that represent regional trends and local materials used during the period of significance, as well as popular architectural styles of that era, including the Early Commercial, Victorian Commercial, Revival styles, and Bungalow/Craftsman styles. The district was listed in the NRHP in 1983, with boundary increases in 1986 and 1997 (Garrison and Woodward 1980; Janus Design 1980; Woodward and others 1985a, 1985b; Woodward and others 1993).

Two contributing properties to the Railroad Historic District were identified within the APE. The Arizona Central Commercial Company Warehouse is on the northeast corner of S. Beaver Street and W. Phoenix Avenue and is a Panel Brick Commercial-style building constructed in 1924 with Moenkopi sandstone. The other contributor within the APE is the Du Beau Motel, which was built in 1929 and is located on the southeast corner of S. Beaver Street and W. Phoenix Avenue. The Du Beau Motel was the first motor hotel built in Flagstaff and represents the beginning of automobile-oriented commercial development in the city. The motel was strategically placed at the intersection of S. Beaver Street and W. Phoenix Avenue, along the original alignment of Route 66. The motel also was included within *The Negro Motorist Green Book*, a travel guide published between 1936 and 1966 to help African-American travelers find businesses willing to serve them during the Jim Crow era, and is significant for its "Green Book" association (Taylor 2021).

The APE also includes three historic highways – Historic Route 66, the Prescott-Jerome Highway/US 89A, and US Highway 89. All three currently share the same alignment and have the appearance of modern, maintained, in-use roadways within the APE. Historic Route 66 was previously determined eligible under NRHP Criteria A and C, and segments of both US 89A and US 89 have been previously determined eligible under Criterion D (Hathaway and Gregory 1991; Newsome 2005, 2007; Spaulding and Lefthand 1995; Thomas and Newsome 2012; Weaver 1990).

The three bridges in the APE include the Phoenix Avenue Bridge (ADOT 08709), the Flagstaff Underpass (ADOT 00529), and the Arizona Mineral Belt Railroad Trestle. The Phoenix Avenue Bridge is on W. Phoenix Avenue, approximately 50 feet west of S. Beaver Street, on the original alignment of Route 66 and crosses the Rio de Flag. The 2-span bridge features a concrete slab superstructure, a concrete deck with asphalt overlay, and a concrete guardrail on its north side with a blind balustrade. The bridge was constructed in 1929 and previously recommended ineligible for the NRHP (FRASERdesign 2008). The Flagstaff Underpass routes the Route 66/US 89/US 89A alignment underneath the BNSF railroad tracks at the intersection of Historic Route 66 and S. Milton Road. This underpass was constructed in 1958 and

appears to have replaced the original underpass constructed during the New Deal era in the 1930s. The steel I-beam stringer bridge features 2 spans and steel baluster guardrails and was previously recommended ineligible for the NRHP (FRASERdesign 2008). The Arizona Mineral Belt Railroad Trestle is within the northeast corner of the Project area, south of the BNSF Railroad tracks. The Arizona Mineral Belt Railroad alignment is no longer extant within the APE, and the City of Flagstaff previously repurposed the trestle as a foot bridge within the Flagstaff Urban Trail system. The trestle was determined to be NRHP eligible by the SHPO as part of cultural resource studies conducted for the Rio de Flag flood control project. A Memorandum of Agreement between USACE and the SHPO signed in June 2021 stipulated that Level II Historic American Engineering Record (HAER) documentation will be completed for the trestle to mitigate adverse effects resulting from that project.

The historic railroad within the APE, is the Atchison, Topeka & Santa Fe Railroad, which was originally constructed as the A&P in 1882 and is currently owned and operated by the BNSF Railway. The railroad alignment was previously determined eligible under NRHP Criteria A and D (Dosh 1996; Newsome 2005, 2007). The railroad within the APE is in-use and maintained, and the only aspect of integrity it retains is location.

Review of Historic Maps and Aerial Photos

GLO township plats, Sanborn Fire Insurance maps, and aerial photos were reviewed to create a timeline of use for the Project area and to gauge for the potential for unrecorded historic-period resources. Timing of GLO cadastral surveys typically reflected the onset of interest in homesteading. The GLO conducted the first cadastral survey of Township 21 North, Range 7 East in 1878.

The plat indicated the township, at the south end of the San Francisco Mountain, was largely covered with pine timber with a few open “parks.” The overland road crossed the township in a generally east-west direction, with two roads branching to the north, including the old Beale road and a road to a spring where a house and sheep pen were mapped and labeled McMillon’s. A dot (perhaps a building) and an adjacent field along the overland road, approximately one-half mile north of the APE, was labeled Flagstaff. The APE is at the southern end of what was mapped the largest open park in the township and less than one-fourth mile east of the overland road where a spring was located (**Figure 8**).

The Project area is depicted on Sanborn maps published in 1892, 1895, 1901, 1910, 1916, and 1948. In 1892, the A&P alignment (later known as the AT&SF and currently as BNSF) and the no longer extant CARY (former Arizona Mineral Belt Railroad) alignment crossed the APE from east to west (**Figure 9**). (The west end of the APE was not mapped by the Sanborn Company in 1892, and the central section was not drawn to scale). A dry creek bed in the location of the present-day Rio de Flag is depicted between the eastern and central sections of the Project area. Trestles carried the A&P and the CARY across the creek bed and a culvert is present on W. Phoenix Avenue. A brick kiln and a small, square, one-story masonry structure also shown in the northwest corner of the central section of the Project area within what is currently a paved parking lot.

By 1895, the brick kiln and the small square building were no longer extant, but the railroad alignments, dry creek bed, and the associated crossings remained unchanged. The 1901 and 1910 maps depict the same features in the Project area. By 1910, the branch of the dry creek bed that continued north toward downtown Flagstaff (see Figure 8) is no longer shown, and the A&P is labeled as the AT&SF.

By 1916, the Rio de Flag had been diverted into the channel formerly labeled as a dry creek bed and the railroad trestles remained (**Figure 10**). The railroad mainline is labeled as the AT&SF and former Arizona Mineral Belt/CARY rail alignment is labeled as a spur to the Arizona Lumber and Timber Company’s saw mill. The eastern section of the Project area east of the Rio de Flag is labeled as a wood and coal yard. Three coal sheds, scales, and a small outbuilding are depicted south of the railroad spur alignment and a railroad pump house and another coal shed are shown north of the spur alignment. A bridge is shown at the W. Phoenix Avenue crossing of the Rio de Flag.

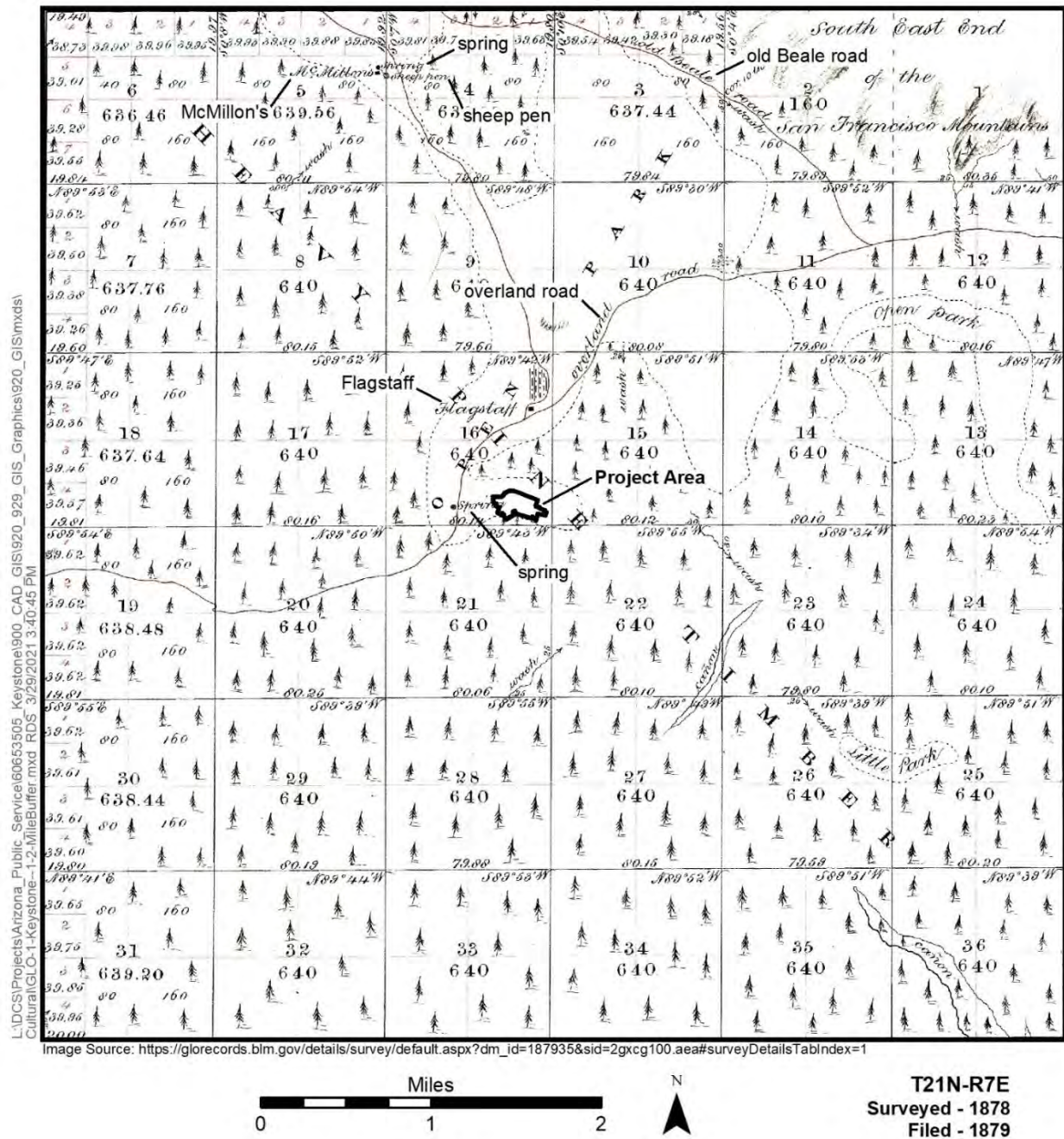


Figure 8. 1879 GLO Plat

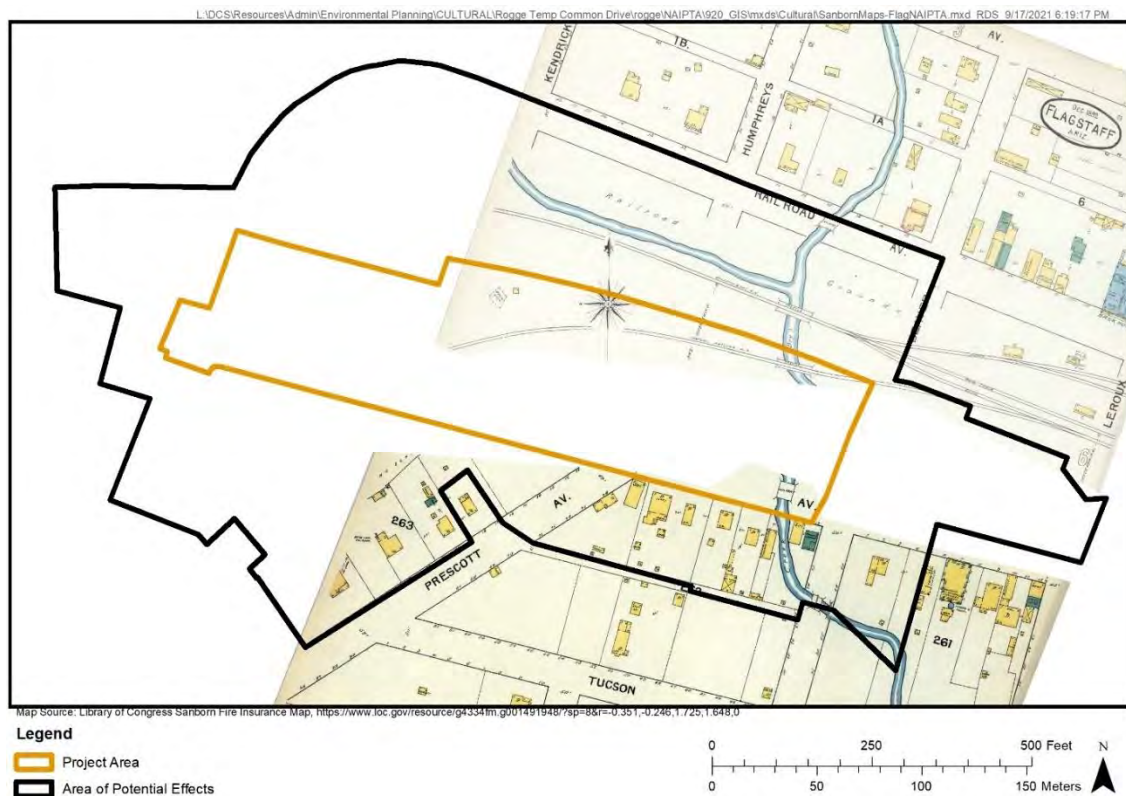


Figure 9. 1892 Sanborn Fire Insurance Map

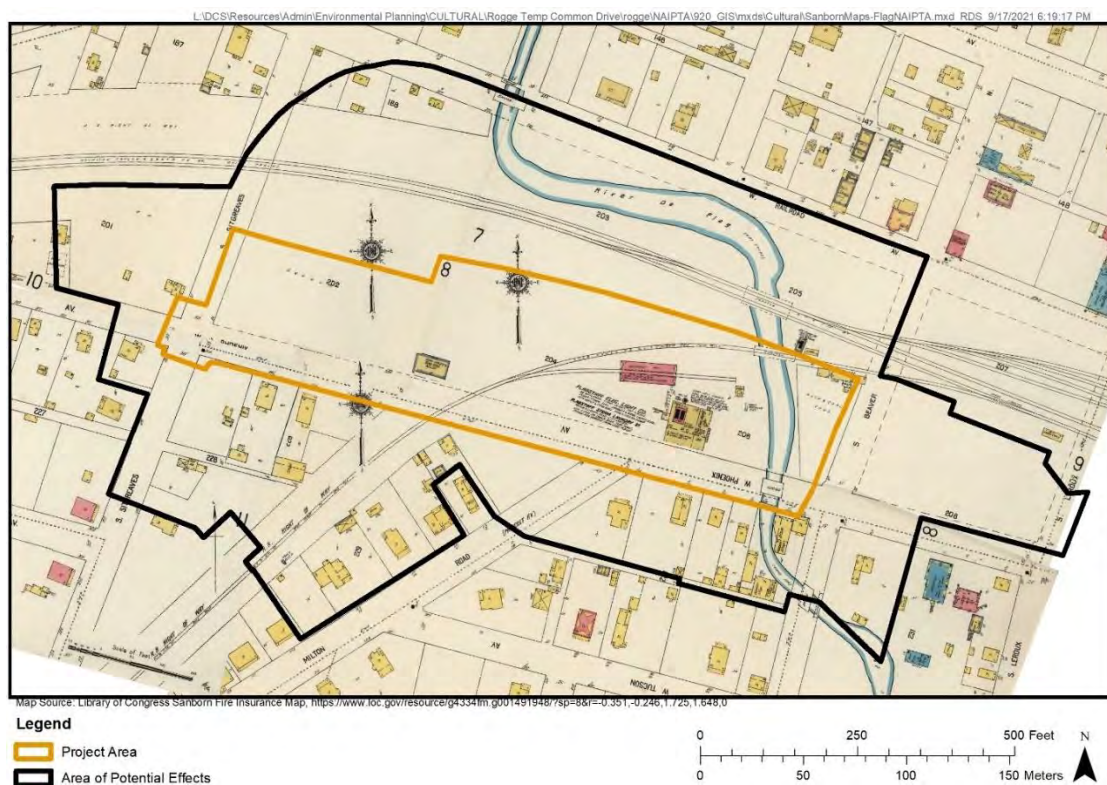


Figure 10. 1916 Sanborn Fire Insurance Map

A two-story masonry building that housed the Flagstaff Electric Light Company is shown within the central section of the Project area south of the railroad spur and west of the Rio de Flag at 120 W. Phoenix Avenue in the eastern section of the Project area (**Figure 11**; see Figure 10). A secondary spur branched from the Arizona Lumber and Timber Company's spur that was used to deliver wood to the plant. A 6-foot-deep sawdust pit shaded by a wood-structure straddles a section of the secondary spur and a conveyor is shown between the pit and the light company building, which contained a furnace, engines, electric repairing storage, and an office. A small storage building and a wood preserving vat are depicted at the east end of the secondary spur. Construction of the light company plant began in 1913 and it began operations in May 1914. The plant, which was located on a parcel leased from the AT&SF, had two large generators and two steam-producing boilers and used sawdust and wood as fuel. The Flagstaff Steam Laundry occupied the second floor of the plant shortly after construction. Between 1919 and 1920, conduits for steam heat were installed to buildings in the area. The system was a closed loop, with condensate returned to the plant through pipes. The steam and return pipes were located within underground conduits approximately 2 feet wide and 3 feet deep. In 1924, the Flagstaff Steam Laundry relocated to a new facility on the southwest corner of W. Phoenix Avenue and Mikes Pike. During a drought in the 1930s, the electric plant used filtered sewage that was piped to the plant and used in the boilers. In 1942, the Arizona Power Company, which was subsequently known as Northern Arizona Light & Power, purchased the Flagstaff Electric Light Company (Griffen 1985; SCS Engineers 2021). The 1916 Sanborn also depicts a 30-by-50-foot iron clad, wood-framed building labeled as a hay, feed, and grocery warehouse is shown the south central section of the Project area, west of Arizona Lumber and Timber Company's spur. No buildings or structures were drawn in the western section of the Project area.

By 1948, the railroad pump house and coal shed north of the spur line were no longer extant north of the eastern section of the Project area (**Figure 12**; see Figure 11). In the central section of the Project area, a rectangular, concrete block building had been constructed east of the Flagstaff Electric Light Company building. The 30-by-50-foot building in the south-central section of the Project area was being used to store fuel and oil and another building had been constructed to the northeast that is labeled as an electric equipment warehouse. The western section of the Project area remained vacant.

By 1950, the introduction of natural gas power had made electricity produced by steam obsolete. The Flagstaff Electric Power Plant continued to produce steam that was distributed by a line originating at the Southwest Lumber Mill until 1966. In 1951, Arizona Public Service (APS) assumed operations at the plant, and between 1956 and 1964 the buildings were used for storage of electrical equipment and materials, including wire, transformers, and creosote poles (Griffen 1985; SCS Engineers 2021). A 1959 aerial photograph indicates the eastern section of the Project area was vacant with the exception of the railroad spur trestle over the Rio de Flag (**Figure 13**). Within the central section, most of the buildings, the saw dust pit, and the railroad spur depicted on the 1948 Sanborn map remained extant and additional buildings are shown on the north side of the railroad spur northwest of the saw dust pit and southeast of the electrical equipment warehouse. The Arizona Distributing Company Warehouse (recorded by this study as Inventory # ML-1), which was constructed in 1955, is visible in the western section of the Project area. Shortly after this warehouse was constructed, an article in the *Arizona Daily Sun* (1956) indicated the building was constructed on the former location of the Arizona Highway Department's road oil tanks.

By the mid-1960s, the eastern section of the Project area was being used as a parking lot. Between 1964 and 1972, APS subleased the property in the central section of the Project area to the City of Flagstaff, which was used by the City's water, sewer, and maintenance departments. The railroad spur appears to have been removed by 1970. In 1972, APS sold their lease back to the AT&SF and sold the buildings and other improvements to the City of Flagstaff. The City of Flagstaff continued to lease the property from the railroad and all buildings and structures were demolished ca. 1980 (SCS Engineers 2021).



(Map Source: SCS Engineers 2021)

Figure 11. Aerial view of the Project Area in 2020 showing locations of the features mapped on the 1916 and 1948 Sanborn maps.

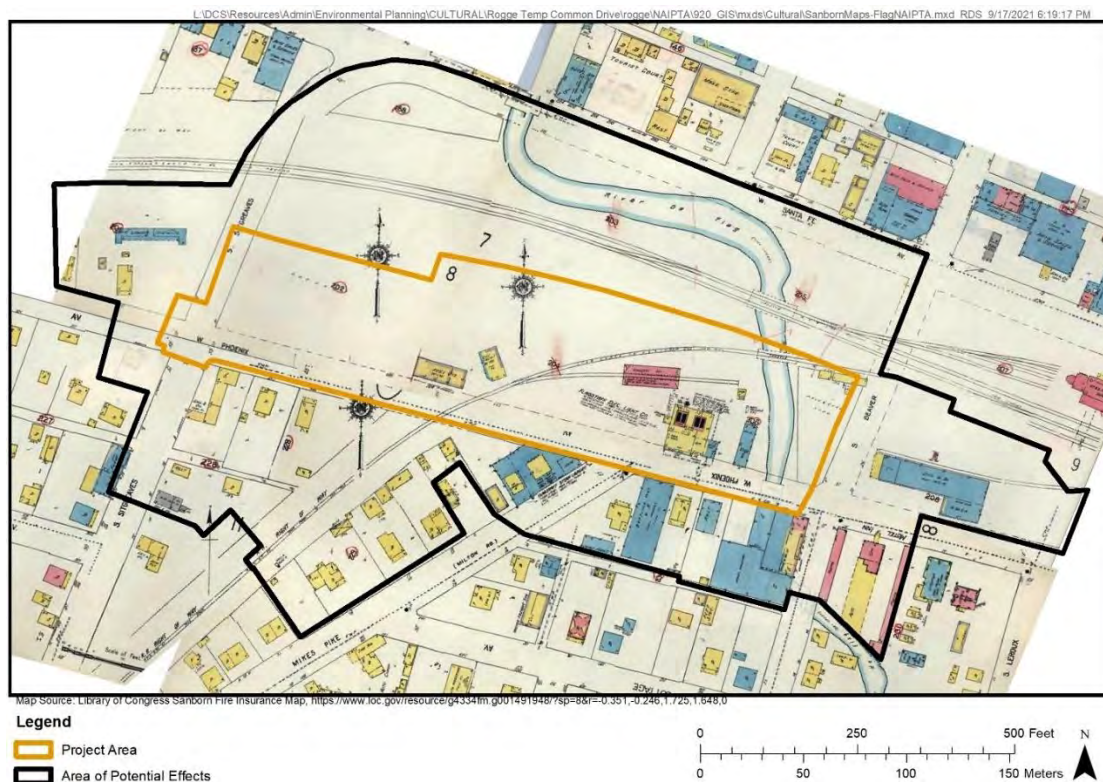


Figure 12. 1948 Sanborn Fire Insurance Map



Figure 13. 1959 aerial photograph.

The City purchased the property from the railroad in 1992. After the City's purchase, the central section has been used as parking lot, the location for a seasonal farmer's market, and a disposal location of snow removed from city streets. The central section of the Project area west of the railroad spur alignment graded and the location of fill soil piles between 1997 and 2003, and the existing DCC and its associated paved parking lot were constructed in 2010. The warehouse on the western section of the Project area continued to be used for beverage distribution until ca. 1992. In 1988, a underground storage tank was installed at the building which was subsequently removed in 1992. The building was occupied by retail home product sales in the 1990s and 2000s. In 2007, the City of Flagstaff purchased the building (SCS Engineers 2021).

Survey Methods and Results

Methods

For purposes of this Project, the historic period was defined to include properties that meet the 50-year threshold of NRHP eligibility at the anticipated project completion in 2025, plus an additional 5-year buffer that allows for unexpected delays in project planning. Accordingly, the historic period was defined as any resource 45 years or older (constructed in 1980 or earlier). The evaluation of historic resources was conducted by historians that meet the Secretary of the Interior's Professional Qualification Standards and was based on the National Park Service (NPS) standards for the identification and evaluation of historic properties, as presented in 36 CFR § 60.4 [a-d].

A field survey was conducted to document historic resources within the APE. Unrecorded historic-period properties in the APE were documented with digital photography that meets the NPS standards for digital photography, and information regarding the location, style, form, details, materials, and construction methods of the historic resource was recorded. Previously unrecorded historic-period properties were identified using records of the Coconino County Assessor's Office and historic maps and aerial photographs. Previously recorded properties in the APE were revisited and photographed to verify their current condition. The previously unrecorded historic-period properties were documented on Arizona State Historic Property Inventory forms and research was conducted to evaluate their eligibility for listing in the NRHP.

A survey for archaeological resource was not conducted because:

- A review of Google imagery and observations made during the August 2021 site visit indicated the Project area is highly modified. There is no visible natural ground surface in the project area and archaeological field survey would not be productive.
- The records review did not identify any previously recorded archaeological sites within 0.5 mile of the Project area indicating the potential for unrecorded sites is limited.
- Bedrock within the Project area is shallow and typically overlain by stoney clay loam, indicating there is little potential for buried archaeological resources.
- A cultural resource for USACE Rio de Flag flood control project covered much of the Project area and recorded no archaeological resources. The only cultural resource recorded was an isolated trestle of the abandoned Arizona Mineral Belt Railroad (USACE 2021).

Survey Results

Five unrecorded historic-period buildings were identified within the APE (see Figure 5). All five resources were identified as commercial property types constructed between 1938 and 1960. The five historic-period properties were recorded in tabular format (**Table 3**) and on Arizona Historic Property Inventory forms (**Appendix B**). The historic property inventory forms provide detailed descriptions, historical information, NRHP evaluations, and photographs for each historic resource.

All five historic-period buildings have been altered since construction by additions, substantial design modifications, and/or altered materials. None of the buildings retain integrity of design, materials, workmanship, feeling, or association, and all are recommended ineligible for individual listing in the NRHP. In addition, none of the buildings are within established NRHP-listed or -eligible historic districts and no potential unidentified historic districts were identified.

Table 3. Previously Unrecorded Historic-Period Buildings in the APE

Inventory #	Historic/Current Name	Address	Description	NRHP evaluation
ML-1	Arizona Distributing Company Warehouse/ City of Flagstaff	216 W. Phoenix Avenue	high-bay one-story utilitarian warehouse/office building constructed in 1955	recommended not eligible
ML-2	Viotti Furniture/Flagstaff Design Center	1 S. Milton Road	1-story commercial box constructed in 1956	recommended not eligible
ML-3	Standard Stations, Inc./ Brake Masters	6 S. Milton Road	1-story post-World-War II oblong box with canopy station constructed in 1960	recommended not eligible
ML-4	Wheeler's Underpass Grocery/Ruff's Sporting Goods	2 S. Milton Road	1-story Commercial-style building constructed in 1938	recommended not eligible
ML-5	Flagstaff Chamber of Commerce/Flagstaff Chamber of Commerce	101 W. Historic Route 66	2-story vernacular building constructed in 1952; remodeled in 1980 and between 2016 and 2018	recommended not eligible

Assessment of Impacts

The records review identified 16 historic properties within the Project APE that have been previously listed or been determined or recommended eligible for the NRHP (**Table 4**). Only one of those properties, the Arizona Mineral Belt Railroad Trestle, is within the Project area; the remaining 15 properties are on adjacent parcels.

The City of Flagstaff previously repurposed the Arizona Mineral Belt Railroad Trestle as a foot bridge within the Flagstaff Urban Trail system. According to the current design, the Project would not result in the alteration or removal of the Trestle, and the expansion of the Mountain Line DCC would not substantially alter the trestle's current viewshed (see **Photos 1 and 2** in **Appendix C**). Therefore, the Project would result in **no adverse effect** to the trestle. The trestle was determined to be NRHP eligible by the SHPO as part of cultural resource studies conducted for the Rio de Flag flood control project. A Memorandum of Agreement between USACE and the SHPO signed in June 2021 stipulated that Level II Historic American Engineering Record (HAER) documentation will be completed for the trestle to mitigate adverse effects resulting from that project.

Table 4. Assessment of Impacts on NRHP-Listed and Eligible Properties

	Historic Name	NRHP Status	Impact
Historic Districts			
1	Flagstaff Southside Historic District	listed, Criteria A and C	no adverse effect
2	Railroad Addition Historic District AZ I:15:53(ASM)	listed, Criteria A and C	no adverse effect
Individual Buildings and In-Use Historic Structures			
3	Arizona Central Commercial Co. Warehouse 2 (24) S. Beaver St.	listed as contributor to the Railroad Addition Historic District	no adverse effect
4	Arizona Machinery Co. Building 7-9 S. Beaver St.	listed as contributor to Flagstaff Southside Historic District	no adverse effect
5	Atchison, Topeka & Santa Fe Railroad Mainline AZ I:14:334 (ASM)	determined eligible under Criteria A and D	no adverse effect
6	Coca-Cola Bottling Works 119 W. Phoenix Ave.	listed as contributor to Flagstaff Southside Historic District	no adverse effect
7	Du Beau Motel 19 W. Phoenix Ave.	listed as contributor to the Railroad Addition Historic District	no adverse effect
8	Flagstaff Steam Laundry 210 W. Phoenix Ave.	listed as contributor to Flagstaff Southside Historic District	no adverse effect

	Historic Name	NRHP Status	Impact
9	Historic Route 66 AZ I:15:156(ASM)	determined eligible, Criteria A and C	no adverse effect
10	Hutchison Building 111 (107) W. Phoenix Ave.	listed as contributor to Flagstaff Southside Historic District	no adverse effect
11	Hutchison & Sauer Building 1-7 S. Beaver St.	listed as contributor to Flagstaff Southside Historic District	no adverse effect
12	Prescott-Jerome Highway/ US 89A, AZ N:7:61(ASM)	segments determined eligible, Criterion D	no adverse effect
13	Marcos Café 113 W. Phoenix Ave.	listed as contributor to Flagstaff Southside Historic District	no adverse effect
14	Arizona Mineral Belt Railroad Trestle	determined eligible, Criterion not specified	no adverse effect
15	T.E. Pulliam House 125 W. Phoenix Ave./12 S. Mike's Pike	recommended eligible as a contributor to the Flagstaff Southside Historic District	no adverse effect
16	US Highway 89 AZ I:3:10(ASM)	segments determined eligible, Criterion D	no adverse effect

The NRHP-listed **Flagstaff Southside Historic District** is south of the Project area, and its seven contributing properties within the APE are on the south side of W. Phoenix Avenue. The Arizona Machinery Company Building faces east toward S. Beaver Street and has no direct view of Project area. The Project area is visible from the other six contributing properties – the Coca Cola Bottling Works, Flagstaff Steam Laundry, Hutchison Building, Hutchison-Sauer Building, Marcos Café, and T.E. Pulliam House (see **Photos 3-12** in Appendix C). However, the existing building on the DCC³ would be replaced with a two-story building that would not be substantially taller, the other improvements associated with the expansion Project would not substantively alter the DCC's current appearance, and the western viewshed from the contributing properties currently features a 5-story apartment complex constructed in 2016. Therefore, the Project would result in **no adverse effect** to the Flagstaff Southside Historic District or its contributors.

The NRHP-listed **Railroad Addition Historic District** is east of the Project area, and its two contributing properties within the APE are on the northeast and southeast corners of W. Phoenix Avenue and S. Beaver Street. Both contributors – the Arizona Central Commercial Co. Warehouse and the Du Beau Motel – face west toward the Project area (see **Photos 13-17** in Appendix C). Most elements of the existing DCC are not visible from the two Railroad Addition Historic District contributors due to mature trees and other mature vegetation, and the improvements associated with the proposed expansion Project would not substantially alter the DCC's current appearance or the viewshed of these two properties. Therefore, the Project would result in **no adverse effect** to the Railroad Addition Historic District or its contributors.

The **AT&SF**, which is currently operated by the BNSF Railway and was previously determined eligible for the NRHP under Criteria A and D, is adjacent to the northern boundary of the Project area (see **Photos 18 and 19** in Appendix C). The current DCC and the locations for the proposed Project expansions are within the current viewshed. However, the railroad within the APE is in-use, maintained, and has the appearance of a modern railroad. Therefore, the proposed Project would not substantially alter the DCC's current appearance or the viewshed of the railroad and would result in **no adverse effect** to the AT&SF.

The historic highways – **Route 66, US 89A and US 89** – share an alignment within the APE, which is north and west of the Project area. This alignment is a major, four-lane thoroughfare within the city of Flagstaff that has been upgraded and improved (see **Photo 20** in Appendix C). Much of the Project area is not visible from the highway alignment due to existing buildings and vegetation. Elements of the proposed Project area that may be visible from the highway alignments would not substantially alter the highways' current viewshed. Therefore, the Project would result in **no adverse effect** to the Route 66, US 89A or US 89 alignment in the APE.

³ Recorded by this study as ML-1 / Arizona Distributing Company Warehouse and recommended ineligible for the NRHP.

Summary and Recommendations

On 12 August 2021, Kirsten Johnson, an AECOM Senior Architectural Historian who meets the Secretary of the Interior Professional Qualification Standards for Historians and Architectural Historians, conducted a field survey of the APE to identify and record historic built environment resources within the APE. The APE was delineated to include the Project area and approximately the adjacent parcels of land, as defined by the Coconino County Assessor. The Project area is southwest of downtown Flagstaff within an urban setting.

Sixteen previously designated historic resources were identified within the APE, including two NRHP-listed historic districts (Flagstaff Southside Historic District and Railroad Addition Historic District); 9 district contributors, 3 historic highways, 1 historic railroad trestle, and 1 historic railroad. Only the historic railroad trestle, an isolated remnant of the Arizona Mineral Belt Railroad, is within the Project area. The USACE Rio de Flag flood control project will likely remove the trestle and the USACE has executed a Memorandum of Agreement stipulating the agency will compile historic documentation and install public signs to mitigate the trestle removal. The current DCC conceptual design indicates the area where the trestle is location would not be modified if the trestle were extant when Phase 2 of the DCC were developed, the Project is unlikely to have an adverse effect on the trestle. The other 15 resources within the APE are outside of the Project area. Although the existing DCC and the proposed Project area are visible or partially visible from most of these resources, the proposed Project would not substantially alter their current viewsheds. Therefore, it is unlikely that the Project would result in any adverse effect on the 16 previously recorded NRHP-listed and eligible resources in the APE.

Five historic-period properties were identified within the APE and recorded on Arizona State Historic Property Inventory forms (see **Appendix B**). All 5 resources were identified as commercial property types constructed between 1938 and 1960. All five historic-period buildings have been altered by additions, substantial design modifications, and/or altered materials. None of the buildings retain integrity of design, materials, workmanship, feeling, or association, and all are recommended ineligible for individual listing in the NRHP. In addition, none of the buildings are within established NRHP-listed or -eligible historic districts and no potential unidentified historic districts were identified.

References

Advisory Council on Historic Preservation

- 2019 Memorandum Re: Recent Court Decision Regarding the Meaning of “Direct” in Sections 106 and 110(f) of the National Historic Preservation Act. Office of General Counsel. Electronic document, https://shpo.nv.gov/uploads/documents/OGC_memo_to_ACHP_staff_re_meaning_of_direct_6-7-19.pdf, accessed September 2021.

Anderson, Bruce A. (compiler)

- 1990 *The Wupatki Archaeological Inventory Survey Project: Final Report*. Professional Paper 35. Southwest Cultural Resources Center, National Park Service, Santa Fe, New Mexico.

Antigua Archaeology

- 2017 *Public Cell Tower Survey*. On file, Arizona State Museum Archaeology Records Office, Tucson, Arizona.

Arizona Archives Online

- 2016 Overview of the City of Flagstaff Records, 1958 Collection. Cline Library, Northern Arizona University, Flagstaff, Arizona. Electronic document, http://www.azarchivesonline.org/xtf/view?docId=ead/nau/city_flagstaff.xml;query=;brand=default, accessed September 2021.

Berry, Claudia F., and Michael S. Berry

- 1986 Chronological and Conceptual Models of the Southwestern Archaic. In *Essays in Honor of Jesse D. Jennings*, edited by Carol J. Condie and Don D. Fowler, pp. 252-327. Anthropological Papers 110. University of Utah, Salt Lake City, Utah.

Bocquet-Appel, Jean-Pierre

- 2011 When the World's Population Took Off: The Springboard of the Neolithic Demographic Transition. *Science* 333:560-561.

Brown, D.E. (editor)

- 1994 *Biotic Communities: Southwestern United States and Northwestern Mexico*. University of Utah Press, Salt Lake City, Utah.

Bungart, Peter W.

- 1990 Late Archaic Expansion in the Upper Glen Canyon Region, Southeastern Utah. Paper presented at 55th Annual Meeting of Society for American Archaeology, Las Vegas, Nevada.

Casey, Dana H.

- 1991 *First Baptist Church National Register of Historic Places Registration Form*. Anthropology Department, Northern Arizona University, Flagstaff, Arizona.

Chambers, Steve

- 1986 *Northern Arizona Normal School Historic District National Register of Historic Places Inventory-Nomination Form*. Office of the Associate Vice President for Administration and Finance, Northern Arizona University, Flagstaff, Arizona.

Childe, V. Gordon

- 1936 *Man Makes Himself*. Watts, London, England.

City of Flagstaff

- 2020 Community Profile. Electronic document: <https://www.flagstaffaz.gov/2/Community-Profile>, accessed September 2021.

- Cleeland, Teri A.
1984 *Ashurst House (U.S. Weather Bureau) National Register of Historic Places Inventory-Nomination Form*. Anthropology Department, Northern Arizona University, Flagstaff, Arizona.
- Cleeland, Teri, and Nancy H. Hoffman
1984 *J.M. Clark House National Register of Historic Places Inventory-Nomination Form*. Northern Arizona University, Flagstaff, Arizona.
- Colton, Harold S.
1960 *Black Sand: Prehistory in Northern Arizona*. University of New Mexico Press, Albuquerque, New Mexico.
- Conway, Roy P.
1986 *South Beaver School National Register of Historic Places Inventory-Nomination Form*. Department of Anthropology, Northern Arizona University, Flagstaff, Arizona.
- Cordell, Linda S., and Maxine E. McBrinn
2012 *Prehistory of the Southwest*. 3rd ed. Left Coast Press, Walnut Creek, California.
- Degher, Darien, and Michael Mongini
2009 *The Ice House National Register of Historic Places Registration Form*. Historic Ice House, LLC., Flagstaff, Arizona.
- Dodge, B.S.
1980 *The Road West: Saga of the 35th Parallel*. University of New Mexico Press, Albuquerque, New Mexico.
- Dosh, Deborah
1996 *A Cultural Resource Survey of the Proposed Railroad Springs Affordable Housing Subdivision, Flagstaff, Coconino County, Arizona*. Project 96-10. Kinlani Archaeology, Flagstaff, Arizona.
- Dosh, Steven G.
2004 *Cultural Resources Survey: Arizona Snowbowl Proposed Reclaimed Water Pipeline Route, Coconino National Forest, Arizona State Trust, and Private Lands in Coconino County, Arizona*. Northland Research, Flagstaff, Arizona.
- Elson, Mark D. (ed)
2011 *Sunset Crater Archaeology: The History of a Volcanic Landscape*. Anthropological Papers 37. Center for Desert Archaeology, Tucson, Arizona.
- FRASERdesign
2008 *Arizona Historic Bridge Inventory*. Loveland, Colorado.
- Garrison, James, and James Woodward
1980 *Railroad Addition Historic District National Register of Historic Places Inventory-Nomination Form*. Janus Design, Tempe, Arizona.
- Geib, Phil R., and Miranda Warburton
1991 *A Class I Cultural Resources and Ethnographic Overview of the Glen Canyon-Shiprock Transmission Line Corridor*. Navajo National Archaeology Department, Window Rock, Arizona.
- Goebel, Ted, Michael R. Waters, and Margarita Dikova
2003 The Archaeology of Ushki Lake, Kamchatka, and the Pleistocene Peopling of the Americas. *Science* 301:501-505.

Griffin, Joyce

- 1985 Electricity and Steam Heat : Co-generation in Flagstaff, 1920-1966. *Journal of Arizona History*. Volume 26, No. 2, pp. 175-202.

Hall, John D.

- 2017 *Revised Class III Cultural Resources Survey of the Proposed AZ10-069 Eagle/AZ2 Aperture Telecommunications Facility, 400 West Elm Avenue, Flagstaff, Coconino County, Arizona*. Project 65177398. Terracon Consultants, Tucson, Arizona.

Hall, S.

- 1906 The Forests of Arizona. *Outwest* 25(6):473.

Hardy, James

- 2018 *Flagstaff's Route 66: Self-Guided Walking Tour*. Flagstaff Visitor Center, Flagstaff, Arizona. Electronic document: https://s3-us-west-1.amazonaws.com/flagstaff-2018/craft/Files/route66_walking.pdf?mtime=20190102145818, accessed September 2021.

Hathaway, Jeffrey B., and Michael M. Gregory

- 1991 *Cultural Resources Survey of 35.85 Mile Segment of U.S. Alternate 89 (US 89A) Right-of-way in Northwestern Yavapai County, Arizona*. Archaeological Research Services, Tempe, Arizona.

Haynes, C. Vance, Jr.

- 2011 Distribution of Clovis Points in Arizona and the Clovis Exploration of the State, 11,000 B.C. *Kiva* 76(3):343-367.

Heng, Bethan, and Abbey Buckham

- 2021 Updated Arizona Historic Property Inventory Forms for Properties within the Flagstaff Southside Historic District. On file, City of Flagstaff Heritage Preservation Office, Flagstaff, Arizona.

Holm, Richard F.

- 2019 *Geology of Flagstaff and Geologic History of Rio de Flag, Northern Arizona, with Trail Guides to Geology Along Rio de Flag*. Arizona Geological Survey, Tucson, Arizona. Electronic document, [dte-23riodeflag-holm-opt.pdf \(az.gov\)](#), accessed 30 August 2021.

Janus Design

- 1980 *Flagstaff, Arizona Historic Property Survey*. Tempe, Arizona.

Larew, Marilyn

- 1977 *Lowell Observatory National Register of Historic Places Inventory-Registration Form*. Historic Sites Survey Division, National Park Service, Washington, D.C.

Lefthand, Nathan J.

- 1994 *An Archaeological Survey of State Route 66, Mileposts 196.25 to 197.18, in the City of Flagstaff, Coconino County, Arizona*. Plateau Mountain Desert Research, Flagstaff, Arizona.
- 1997 *An Archaeological Survey of Business-40 Right-of-way between Milepost 194.75 and 195.53 in Flagstaff, Coconino County, Arizona*. Plateau Mountain Desert Research, Flagstaff, Arizona.

Lonardo, Cara

- 2006 *A Cultural Resources Survey of State Route 40 Business between Mileposts 193.2 and 197.2, Flagstaff, Coconino County, Arizona*. Technical Report 065026. Logan Simpson Design, Tempe, Arizona.

Mabry, Jonathan B. (ed.)

- 1998 *Paleoindian and Archaic Sites in Arizona*. Technical Report 97-7. Center for Desert Archaeology, Tucson, Arizona.

- McGregor, John C.
1936 *Culture of Sites Which Were Occupied Shortly Before the Eruption of Sunset Crater*. Bulletin 9. Museum of Northern Arizona, Flagstaff, Arizona.
- McGregor, John C.
1937 *Winona Village*. Bulletin 12. Museum of Northern Arizona, Flagstaff, Arizona.
- Michael Wilson Kelly Architects
2009 *Flagstaff Southside Historic District National Register of Historic Places Registration Form*. Tempe, Arizona.
- Minnis, Paul E.
1989 Prehistoric Diet in the Northern Southwest: Macroplant Remains from Four Corners Feces. *American Antiquity* 54(3): 543-563.
- Morris, Rick, Monique E. Kimball, Thomas F. Messerli, and Harding Polk II
1994 *Across the Colorado Plateau, Anthropological Studies for the Transwestern Pipeline Expansion Project, Volume 19: Hot Nights, San Francisco Whiskey, Baking Powder, and a View of the River, Life on the Southwestern Frontier*. Project 185-461B. Office of Contract Archaeology, University of New Mexico, Albuquerque, New Mexico.
- Natural Resource Conservation Service
2021 Web Soil Survey. Electronic document, <https://websoilsurvey.sc.egov.usda.gov/App/HomePage.htm>, accessed September 2021.
- Neal, Lynn A.
2005 *Cultural Resources Evaluation for Northern Arizona University's South San Francisco Street Sidewalk Project, Flagstaff, Coconino County, Arizona*. Report 05.1077. EnviroSystems Management, Flagstaff, Arizona.
- Newsome, Daniel K.
2005 *Cultural Resources Evaluation for the West Village to Rio de Flag Urban Trail in West Flagstaff, Coconino County, Arizona*. ADOT Project TEA-FLA-0-(005)A. ADOT TRACS No. 000 CN-FLA-SL549 01L. EnviroSystems Management, Flagstaff, Arizona.
- 2007 *Cultural Resources Evaluation for the Rio de Flag Wash to San Francisco Street Urban Trail Segment in Downtown Flagstaff, Coconino County, Arizona*. ADOT Project TEA-FLA-0-(006)A. ADOT TRACS SL592 01C. EnviroSystems Management, Flagstaff, Arizona.
- Olberding, Susan
2014a Flagstaff grows up: 1920-1970s. *Arizona Capital Times*. Electronic document: <https://azcapitoltimes.com/news/2014/07/18/flagstaff-grows-up-1920s-1970s/>, accessed September 2021.
- 2014b Modern Flagstaff. *Arizona Capital Times*. Electronic document: <https://azcapitoltimes.com/news/2014/08/15/modern-flagstaff/>, accessed September 2021.
- Paradis, Thomas W.
2003 *Theme Town: A Geography of Landscape and Community in Flagstaff, Arizona*. iUniverse, Lincoln, Nebraska.
- Peterson, Kenneth Lee
1981 10,000 Years of Climate Change Reconstructed from Fossil Pollen, La Plata Mountains, Southwestern Colorado. Ph.D. Dissertation, Washington State University, Washington.

Pilles, Peter J., Jr.

- 1979 Sunset Crater and the Sinagua: A New Interpretation. In *Volcanic Activity and Human Ecology*, edited by P. Sheets and D.K. Grayson, pp. 459-485. Academic Press, New York.

Pilles, Peter J., Jr., and Pat H. Stein

- 1981 *A Cultural Resources Overview of the Coconino National Forest*. Coconino National Forest, Flagstaff, Arizona.

Politis, Gustavo G., and Luciano Prates

- 2019 The Pre-Clovis Peopling of South America. *SAA Archaeological Record* 19(2):40-44.

Pritchard, Nancy, and Steven Strand

- 1984 *Milligan House National Register of Historic Places Inventory-Nomination Form*. History Department, Northern Arizona University, Flagstaff, Arizona.

Reid, Jefferson, and Stephanie Whittlesey

- 1997 *The Archaeology of Ancient Arizona*. University of Arizona Press, Tucson, Arizona.

SCS Engineers

- 2021 *Phase I Environmental Site Assessment Flagstaff Downtown Connection Center 116 and 216 West Phoenix Avenue APNs 100-43-003B, 100-43-001D. and 100-44-005A Flagstaff, Arizona*. Tempe, Arizona.

Smiley, Francis E.

- 2002 The First Black Mesa Farmers: The White Dog and Lolomai Phases. In *Prehistoric Culture Change on the Colorado Plateau: Ten Thousand Years on Black Mesa*, edited by Shirley Powell and Francis E. Smiley, pp. 37-66. University of Arizona Press, Tucson.

Spaulding, Nathanael E., and Nathan J. Lefthand

- 1995 *An Archaeological Survey of a Portion of US 89 Right-of-Way, from Milepost 465.39 to 470.8, North and South of Cameron, Coconino County, Arizona*. Plateau Mountain Desert Research, Flagstaff, Arizona.

Spicer, Edward H.

- 1962 *Cycles of Conquest: The Impact of Spain, Mexico, and the United States on the Indians of the Southwest, 1533-1960*. University of Arizona Press, Tucson, Arizona.

Southard, John Larsen

- 2013 Riches, Ruin, and Recovery: The Impact of Route 66 on Flagstaff, 1926 to 1938. *The Journal of Arizona History*. Volume 54, No. 2 (Summer 2013), pp. 153-174.

Stein, Pat H.

- 1993 *Logging Railroads of the Coconino and Kaibab National Forests: Supplemental Report to a National Register of Historic Places Multiple Property Documentation Form*. Report 93-16. SWCA Environmental Consultants, Flagstaff, Arizona.

Taylor, Candacy

- 2021 *Route 66 and the Negro Motorist Green Book*. Electronic document, <https://www.moon.com/travel/road-trips/route-66-the-negro-motorist-green-book/>, accessed September 2021.

Thomas, Anne R., and Daniel K. Newsome

- 2012 *Cultural Resources Evaluation for the Flagstaff Urban Trail System Signage Project, Coconino County, Arizona*. Report 1603-11. EnviroSystems Management, Flagstaff, Arizona.

Thurtle, Mary Charlotte.

- 2001 *A Class III Cultural Resource Survey of a Cement Plant Site at 633 East Brannen, Flagstaff, Coconino County, Arizona*. Cultural Resource Report 01-81. SWCA Environmental Consultants, Tucson, Arizona.

Trennert, Robert A.

- 1970 A Vision of Grandeur: The Arizona Mineral Belt Railroad. *Arizona and the West*. Winter, Vol. 12, No. 4, pp. 339-354.

Turner, Korri

- 2007 *A Class III Cultural Resources Survey of Approximately 1.74 acres within the City of Flagstaff, Coconino County, Arizona*. Project Report 2007-005. Carter & Burgess, Phoenix, Arizona.

US Army Corps of Engineers

- 2000 *Rio de Flag, Flagstaff, Arizona, Feasibility Report and Final Environmental Impact Statement*. Los Angeles District, Los Angeles, California.
- 2021 Memorandum of Agreement between the Los Angeles District, U.S. Army Corps of Engineers and the Arizona State Historic Preservation Officer Pursuant to 36 CFR Part 800 Regarding the Rio de Flag Flood Control and Recreation Project, Flagstaff, Coconino County, Arizona. Los Angeles District, Los Angeles, California.

Waters, Michael R.

- 2019 Early Exploration and Settlement of North American during the Late Pleistocene. *SAA Archaeological Record* 19(2):34-39.

Weaver, Donald E., Jr.

- 1991 *An Archaeological Survey of an Access Route Parcel for the Padre Canyon Bridges along I-40 East of Winona, Coconino County, Arizona*. Plateau Mountain Desert Research, Flagstaff, Arizona.

Western Regional Climate Center

- 2021 Flagstaff 4 SW, Arizona (023009). Electronic document, <https://wrcc.dri.edu/cgi-bin/cliMAIN.pl?az3009>, accessed September 2021.

Wilcox, David R.

- 1986 Interim Report: Excavation of Three Sites on Bottomless Pits Mesa, Flagstaff, Arizona. Manuscript on file, Coconino National Forest, Flagstaff.

Woodward, James W.

- 1985 *North End Historic Residential District National Register of Historic Places Inventory-Nomination Form*. Janus Associates, Phoenix, Arizona.

Woodward, James W., Deborah House, and Pat Stein

- 1985a *Flagstaff Multiple Resource Area National Register of Historic Places Inventory-Nomination Form*. Janus Associates, Phoenix, Arizona.
- 1985b *Flagstaff Historic Property Survey*. Janus Associates, Phoenix, Arizona.
- 1985c *Flagstaff Townsite Historic Residential District National Register of Historic Places Inventory-Nomination Form*. Janus Associates, Phoenix, Arizona.

Woodward, James W., Patricia A. Osmon, and N. Christine Richards.

- 1993 *City of Flagstaff Southside/Old Town Historic Building Survey*. Volumes I and II. Woodward Architectural Group, Tempe, Arizona.

Appendix A: Historic Buildings and Structures within the 0.5-Mile Buffer

	Property Name	Description	NRHP Status	Reference
Historic Districts				
1	Flagstaff Townsite Historic Residential District	residential district roughly bounded by Toltec St. on the west, Humphreys St. on the east, Santa Fe Ave. on the south, and Cherry St. on the north; 96 contributing properties	listed, Criteria A and C	Woodward and others 1985a, 1985b, 1985c
2	North End Historic Residential district	residential district roughly bounded by Humphreys St. on the west, Verde St. on the east, Hunt St. on the north, and Cherry St. and Elm St. on the south; 68 contributing properties	listed, Criteria A and C	Woodward and others 1985a, 1985b; Woodward 1985
3	Northern Arizona Normal School Historic District	14-acre area on Northern Arizona University campus; largest and best-preserved historic sandstone structures in Arizona; 10 contributing properties	listed, Criteria A and C	Chambers 1986
Individually Listed Buildings				
4	Ashurst (Henry F.) Home, AZ I:14:32(ASM)	Queen Anne-style residence constructed in 1897	individually listed, Criteria A, B, and C; listed as a contributor to the Flagstaff Townsite Historic District	Janus Design 1980; Cleeland 1984; Woodward and others 1985a, 1985b, 1985c
5	Bank Hotel/McMillan Building, AZ I:14:50(ASM)	hotel constructed in 1888 with Victorian-style influences using local red sandstone; oldest intact structure in Flagstaff	individually listed, Criterion C; listed as a contributor to the Railroad Addition Historic District	Garrison and Woodward 1980; Janus Design 1980; Woodward and others 1985a, 1985b
6	Brannen-Divine House, 209 E. Cottage	Queen Anne-style residence constructed in 1892	individually listed, Criterion C; listed as contributor to the Flagstaff Southside Historic District	Woodward and others 1985a, 1985b; Woodward and others 1993; Michael Wilson Kelly Architects 2009
7	C&M Garage/Double Circle Garage, 204 S. Mikes Pike	Commercial-style building constructed in 1926	individually listed, Criteria A and C; listed as contributor to the Flagstaff Southside Historic District	Woodward and others 1985a, 1985b; Woodward and others 1993; Michael Wilson Kelly Architects 2009
8	H.E. Campbell House, 215 N. Leroux	Queen Anne-style residence constructed in 1890	individually listed, Criteria B and C	Woodward and others 1985a, 1985b
9	J.M. Clark House, 503 N. Humphreys St.	Craftsman Bungalow-style residence constructed in 1911	individually listed, Criterion C; listed as contributor to the North End Historic District	Janus Design 1980; Cleeland and Hoffman 1984; Woodward 1985
10	First Baptist Church, 123 S. Beaver St.	Gothic Revival-style church constructed between 1939 and 1940	individually listed, Criterion C and Criteria Consideration A; listed as a contributor to the Flagstaff Southside Historic District	Woodward and others 1985b; Casey 1991; Woodward and others 1993; Michael Wilson Kelly Architects 2009

	Property Name	Description	NRHP Status	Reference
11	Flagstaff Armory, 503 S. Milton Rd.	Second Renaissance Revival-style armory constructed in 1920	individually listed, Criterion A	Woodward and others 1985a, 1985b
12	House at 310 S. Beaver St.	Bungalow-style residence constructed between 1910 and 1916	individually listed, Criterion C; listed as contributor to the Flagstaff Southside Historic District	Woodward and others 1985a, 1985b; Woodward and others 1993; Michael Wilson Kelly Architects 2009
13	House at 720 Grand Canyon Ave.	Queen Anne-style residence constructed between 1895 and 1901	individually listed, Criteria B and C	Woodward and others 1985a, 1985b
14	The Ice House, 201 E. Birch Ave.	warehouse constructed of prehistoric Kaibab stone in 1947	individually listed, Criteria A and C	Degher and Mongini 2009
15	La Ciudad de Mexico Grocery, 217 S. San Francisco St.	Colonial Revival-style building constructed in 1923	individually listed, Criterion C; listed as contributor to the Flagstaff Southside Historic District	Woodward and others 1985a, 1985b; Woodward and others 1993; Michael Wilson Kelly Architects 2009
16	La Iglesia Metodista Mexicana, 319 S. San Francisco St.	Gothic Revival-style church constructed in 1891	individually listed, Criteria A and C; listed as contributor to the Flagstaff Southside Historic District	Janus Design 1980; Woodward and others 1985a, 1985b; Woodward and others 1993; Michael Wilson Kelly Architects 2009
17	J.C. Milligan House, 323 W. Aspen Ave.	Queen Anne-style residence constructed ca. 1904	individually listed, Criteria B and C	Janus Design 1980; Prichard and Strand 1984; Newsome 2005
18	Lowell Observatory, AZ I:14:33(ASM)	observatory and associated buildings and structures built between ca. 1894 to 1914	National Historic Landmark; individually listed, Criteria B and C	Larew 1977
19	Our Lady of Guadalupe Church, 302 S. Kendrick St.	Gothic Revival-style church constructed in 1926	individually listed, Criteria A and C; listed as contributor to the Flagstaff Southside Historic District	Woodward and others 1985a, 1985b; Woodward and others 1993; Michael Wilson Kelly Architects 2009
20	Presbyterian Church Parsonage, 15 E. Cherry St.	Queen Anne-style residence constructed in 1893	individually listed, Criterion C	Woodward and others 1985a, 1985b
21	Prochnow House, 304 S. Elden St.	Colonial Revival-style residence constructed ca. 1908	individually listed, Criterion C; listed as contributor to the Flagstaff Southside Historic District	Woodward and others 1985a, 1985b; Woodward and others 1993; Michael Wilson Kelly Architects 2009
22	South Beaver School, 506 S. Beaver St.	2-story school constructed of Malpais stone in 1935	individually listed, Criteria A and C	Conway 1986
23	Weatherford Hotel, AZ I:14:26(ASM)	hotel constructed between 1898 and 1899 with Victorian-style influences using local sandstone	individually listed, Criterion A; listed as a contributor to the Railroad Addition Historic District	Garrison and Woodward 1980; Janus Design 1980; Woodward and others 1985a, 1985b

Appendix B: Arizona State Historic Property Inventory Forms

STATE OF ARIZONA

HISTORIC PROPERTY INVENTORY FORM

Please type or print clearly. Fill out each applicable space accurately and with as much information as is known about the property. Use **continuation sheets where necessary**. Send completed form to: State Historic Preservation Office, 1100 W. Washington, Phoenix, AZ 85007

PROPERTY IDENTIFICATION

For properties identified through survey: Site No: ML-1 Survey Area: Mountain Line DCC

Historic Name(s): Arizona Distributing Company Warehouse
(Enter the name(s), if any, that best reflects the property's historic importance.)

Address: 216 W. Phoenix Avenue

City or Town: Flagstaff ☐ vicinity County: Coconino Tax Parcel No. 100-43-003B

Township: 21N Range: 7E Section: 16 Quarter Section: SE1/4 Acreage: 0.91

Plat (Addition): Burlington Northern & Santa Fe Railway Co. Station Ground Property

Block: N/A Lot(s): N/A Year of plat (addition): N/A

Latitude: 35.197525 Longitude: -111.653294 USGS 7.5' quad map: Flagstaff West

Style: utilitarian with Modern details

Architect: _____ ☒ not determined ☐ known (source: _____)

Builder: Mardian Construction Company ☐ not determined ☒ known (source: Arizona Daily Sun 1956)

Construction Date: 1955 ☒ known ☐ estimated (source: Arizona Daily Sun 1956)

STRUCTURAL CONDITION

☒ Good (well maintained, no serious problems apparent)

☐ Fair (some problems apparent) Describe: _____

☐ Poor (major problems; imminent threat) Describe: _____

☐ Ruin/Uninhabitable

USES/FUNCTIONS

Describe how the property has been used over time, beginning with the original use.

beverage distribution warehouse

furniture retail store

city offices and meeting rooms

Sources: Arizona Daily Sun 1956,
1994, 2007

PHOTO INFORMATION

Date of photo: 12 August 2021

View Direction (looking towards)
northwest



SIGNIFICANCE

To be eligible for the National Register of Historic Places, a property must represent an important part of the history or architecture of an area. Note: a property need only be significant under one of the areas below to be eligible for the National Register.

- A. HISTORIC EVENTS/TRENDS (On a continuation sheet describe how the property is associated either with a significant historic event, or with a trend or pattern of events important to the history of the nation, the state, or a local community.)
- B. PERSON (On a continuation sheet describe how the property is associated with the life of a person significant in the past.)
- C. ARCHITECTURE (On a continuation sheet describe how the property embodies the distinctive characteristics of a type, period, or method of construction, or that represents the work of a master, or possesses high artistic values.)

Outbuildings: (Describe any other buildings or structures on the property and whether they may be considered historic.)
none

INTEGRITY

To be eligible for the National Register, a property must have integrity, that is, it must be able to visually convey its importance. Provide detailed information below about the property's integrity. Use continuation sheets if necessary.

1. LOCATION ☒ Original Site ☐ Moved (date _____) Original Site: _____
2. DESIGN (Describe alterations from the original design, including dates—known or estimated—when alterations were made)
West side of original façade and vertical window opening on the east side of the façade infilled at an undetermined date; additions to the west and rear (south) elevations constructed between 1965 and 1980; fenestration patterns altered on the façade of the west addition, windows and doors replaced throughout, vehicle bay infilled on the east elevation – dates undetermined.
3. SETTING (Describe the natural and/or built environment around the property): The surrounding area includes the BNSF railroad and downtown Flagstaff to the north, and commercial and multi-family residential properties to the south, east, and west. A paved parking lot associated with the existing Downtown Connection Center is adjacent to the east.
Describe how the setting has changed since the property's period of significance: Although the building's setting has not changed substantially since its construction in 1955, the construction of a 5-story apartment building on the south side of West Phoenix Avenue in 2016 changed the building's south-facing viewshed and diminished its integrity of setting.
4. MATERIALS (Describe the materials used in the following elements of the property)
Walls (structure): variegated concrete block Foundation: concrete Roof: built-up material
Windows: aluminum sash, sliding, and fixed units
If the windows have been altered, what were they originally? undetermined
Wall Sheathing: painted structural concrete block; rusticated concrete block; T1-11 siding
If the sheathing has been altered, what was it originally? originally unpainted concrete block; glass curtain walls and masonry pilasters
5. WORKMANSHIP (Describe the distinctive elements, if any, of craftsmanship or method of construction)
The building is an example of a utilitarian warehouse building that originally featured Modern façade details including the low-pitched gabled roof, the glass curtain walls separated by brick pilasters, and the tall corner parapet designed for signage. Although the gabled roof and the tall corner parapet remain intact, the façade had been infilled with non-historic materials and the building is a modest and unremarkable example of its type.

NATIONAL REGISTER STATUS (if listed, check the appropriate box)

☐ Individually listed; ☐ Contributor ☐ Noncontributor to _____ Historic District
Date Listed: _____ Determined eligible by Keeper of National Register (date: _____)

RECOMMENDATIONS OF ELIGIBILITY (opinion of SHPO staff or survey consultant)

Property ☐ is ☒ is not eligible individually.
Property ☐ is ☒ is not eligible as a contributor to a potential historic district.
☐ More information needed to evaluate.

If not considered eligible, state reason: Substantially compromised integrity and lack of significance.

FORM COMPLETED BY

Name and Affiliation: Kirsten Johnson, AECOM
Mailing Address: 7720 N. 16th Street, Ste. 100, Phoenix, Arizona 85020

Date: 17 August 2021
Phone No.: (602) 861-7413

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Arizona Distributing Company Warehouse

Continuation Sheet No.1

Site ML-1 is on a 0.91-acre parcel on the northeast corner of South Milton Road and West Phoenix Avenue in Flagstaff. Historic and modern aerial photographs show the site contains one commercial historic resource. Coconino County Assessor's data indicates a construction year of 1971. However, the building is visible on a 1964 aerial photograph, and a 1956 newspaper article documents the year of construction as 1955 (*Arizona Daily Sun* 1956). Field observation confirmed the accuracy of the 1955 year of construction of the original section of the building, and the 1971 date provided by the assessor likely refers to the construction of a later addition. None of the additions are present on the 1964 aerial photograph, but all are shown on 1980 aerial photograph.

Site ML-1 is a high-bay one-story, reinforced concrete building with a concrete foundation. The building has an L-shaped plan composed of the original rectangular footprint with additions on the west and rear (north) elevations. All roof surfaces appear to be clad with built-up material. The exterior walls are primarily exposed, structural, 4-inch concrete block that have been painted. Sections of peeling paint and a historic photograph suggest that the concrete blocks were originally variegated (*Arizona Daily Sun* 1956).

The building faces south, and the façade of the original structure features a low-pitched front-gabled roof and is separated into two sections. The west half is inset and shaded by the overhanging eaves. The lower level includes a row of six aluminum-framed fixed and two-part sliding windows with projecting exterior vinyl-framed storm windows and a single-entry metal flush door accessed by a wooden ramp with wood railing. The upper level includes a row of four aluminum-framed fixed and sliding windows with exterior storm windows that may serve as clerestory windows to a high-bay one-story interior space or an interior mezzanine level. The exterior walls are clad with T1-11 siding with rusticated brick below the lower level windows. A historic photograph indicates the west half of the façade originally featured glass curtain walls separated by six masonry pilasters, and the T1-11 siding, rusticated brick, and both the lower and upper level windows are non-historic infill (*Arizona Daily Sun* 1956). The east half of the original façade features a minimal eave overhang and a full-height inset near the southeast corner that appears to have originally contained a vertical row of windows that has been subsequently infilled with concrete block.

The east elevation of the original structure features an extended rectangular parapet on the southeast corner that historically included signage for the associated business. The remainder of the east elevation has a shorter roof parapet. Fenestration on the east elevation includes a non-historic, two-part aluminum sliding window; a former vehicle bay infilled with T1-11 siding and a single-entry metal glazed and paneled door; and a non-historic anodized aluminum storefront system composed of a single-entry door with transom lights and sidelights. The west and north elevations of the original structure have been concealed by the subsequent additions.

Three additions have been constructed to the rear (north) and west elevations of the original structure. The addition constructed on the west elevation is a high-bay, one-story structure with a higher roof profile than the original

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Arizona Distributing Company Warehouse

Continuation Sheet No.2

section of the building. This addition features a flat roof with parapets on its east elevation and façade (south) and an overhang on its west elevation. The façade currently includes a one-over-one, aluminum sash window and a tripartite window composed of a central fixed window flanked by sliding windows. These windows also feature projecting, exterior, vinyl-framed storm windows. The rest of the façade lacks fenestration, but patterns in the concrete block suggest that multiple openings have been infilled. The west elevation of this addition includes three fixed windows, a single-entry aluminum and glass storefront door, and a vehicle bay with an aluminum roll-up door. The addition's east elevation is concealed by the original section of the building and its rear (north) elevation by the north addition.

The north addition was constructed on the rear elevations of the original structure and the west addition. Its roof is flat with parapets. Its east elevation features an inset vehicle bay with an aluminum roll-up door and an adjacent concrete loading dock. Overgrowth of vegetation indicates the vehicle bay and loading dock are no longer in use. The north addition's rear (north) elevation lacks fenestration and architectural detail. Another addition was constructed on the west elevation of the north addition, and its roof is also flat with parapets. Its façade (south) includes a vehicle bay with an aluminum roll-up door, a single-entry metal flush door, and raised vehicle bay with an aluminum roll-up door used as a loading dock, and its west and rear (south) elevations appear to lack fenestration and architectural detail.

ML-1 was constructed in 1955 as the Flagstaff branch office of the Arizona Distributing Company, a wholesale liquor distributor founded in the 1940s by Milton W. Odom, and his brothers-in-law, Charles J. Minning and John T. Braddock (*Arizona Republic* 2006; *Tucson Citizen* 1991). At the time of the company's founding, Mr. Odom also operated Odom Corporation, one of Alaska's leading wholesale meat, produce, soft drink, and liquor distributors (Gerhart 2019). The Arizona Distributing Company at one time was the largest wholesale liquor business in Arizona, and by 1953, the Arizona Distributing Company had warehouses in Phoenix, Flagstaff, Yuma, and Globe, and plans were underway to construct a new warehouse in Tucson (*Arizona Daily Star* 1953; *Arizona Daily Sun* 1953; *Tucson Citizen* 1991). The company's first warehouse in Flagstaff was shared with the Swift Company and located at 133 East Aspen Avenue (*Arizona Daily Sun* 1953). In June 1955, the company began construction of a new warehouse (ML-1) on property owned by the Santa Fe Railroad at 216 West Phoenix Avenue that had formerly been the location of the Arizona State Highway Department's road oil tanks. The new facility included a large warehouse and a sheltered garage and was constructed at a cost of \$60,000, with the Mardian Construction Company of Phoenix serving as the general contractor. In 1956, the company headquarters was located at 1310 North 22nd Avenue in Phoenix. The Tucson warehouse was designated as the number two office, Flagstaff as number three, and Casa Grande as sixth. Most liquor distributed by the company was delivered to the warehouses by rail, and then distributed by truck both locally and throughout the state (*Arizona Daily Sun* 1956). The Arizona Distributing Company operated a recycling center within its warehouse in the 1970s and

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Arizona Distributing Company Warehouse

Continuation Sheet No.3

continued to occupy the building at least through the 1983 (*Arizona Daily Sun* 1975, 1983). In the 1990s, the Home Alternatives woodstove retail store occupied the building, and the City of Flagstaff purchased the building in 2007 (*Arizona Daily Sun* 1994, 2007). The building currently houses office spaces and public meeting rooms. The existing Downtown Connection Center parking lot is adjacent to the east.

The Arizona Distributing Company Warehouse has not been moved and retains integrity of location. The building is within an urban area within the city of Flagstaff, and the surrounding area includes the BNSF railroad and downtown Flagstaff to the north, and commercial and multi-family residential properties to the south, east, and west. Although the building's setting has not changed substantially since its construction in 1955, the construction of a 5-story apartment building on the south side of West Phoenix Avenue in 2016 changed the building's south-facing viewshed and diminished its integrity of setting. The infill of the west half of the original façade, multiple additions, and altered materials, including replacement windows and doors, have substantially compromised the building's integrity of design, materials, workmanship, feeling, and association. Although the additions were also constructed during the historic period defined for this study (prior to 1981), the fenestration patterns on the west addition have been modified and all are examples of utilitarian warehouse structures that do not possess distinctive architecture.

Although Site ML-1 is associated with the commercial development of Flagstaff in the 1950s and the Arizona Distributing Company, the building lacks historic integrity. The company was at one time the largest wholesale liquor distributor in Arizona, but the Flagstaff warehouse did not serve as the company headquarters and was one of many such warehouses the company constructed in key cities statewide. Historical research did not identify any associations with historical events or persons in history that rise to the level of significance needed to qualify for eligibility under NRHP Criterion A or B. The building is an example of a utilitarian warehouse building that originally featured Modern façade details including the low-pitched gabled roof, glass curtain walls separated by brick pilasters, and the tall corner parapet designed for signage. Although the gabled roof and the tall corner parapet remain intact, the curtain wall on the façade has been infilled with non-historic materials. The building is a modest and unremarkable example of its type and the building does not qualify for eligibility under NRHP Criterion C. Furthermore, the resource is not likely to yield information important to history or prehistory, and does not qualify for NRHP eligibility under Criterion D. Therefore, Site ML-1 is recommended not eligible for listing in the NRHP.

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Arizona Distributing Company Warehouse Continuation Sheet No.4

REFERENCES

Arizona Daily Star. 1953. Plans for Warehouse Costing \$70,000 Told by Distributing Firm. 8 July, p. 16.

Arizona Daily Sun. 1953. Burglars Enter Liquor, Ice Cream Warehouses. 18 May, p. 2.

_____. 1956. Distributing Firm Occupies New \$60,000 Building Here. 22 February, p. 4.

_____. 1975. Residents Keep Recyclers Busy. 21 January, p. 6.

_____. 1983. Council to Consider Final Approval of Fairfield Plan. 28 February, p. 3.

_____. 1994. Advertisement for Home Alternatives. 5 November, p. 20.

_____. 2007. Legal Notices – Legal No. 9135. 28 March, p. 27.

Arizona Republic. 2006. Obituary for Charlie Minning. 17 March, p. 35.

Gerhart, Clifford. 2019. Alaska Business Hall of Fame Laureate: Milton W. Odom. *Alaska Business*. Electronic document, <https://www.akbizmag.com/industry/nonprofits/alaska-business-hall-of-fame-laureate-milton-w-odom/>, accessed August 2021.

Tucson Citizen. 1991. Obituary for Braddock, John T., Jr. 7 January, p. 28.

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Arizona Distributing Company Warehouse

Continuation Sheet No.5

PHOTOGRAPHS



LIQUOR OFFICE — Arizona Distributing Company's new \$60,000 office, located at 216 W. Phoenix here, is one of Flagstaff's major wholesale plants. E. A. Buckman is branch manager.

Oblique view of the west elevation and façade of Site ML-1 in 1956, looking northeast (*Arizona Daily Sun* 1956).



View of the façade of Site ML-1, looking northeast.

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Arizona Distributing Company Warehouse

Continuation Sheet No.6



Façade view of Site ML-1, looking northeast.



View of the east elevation of ML-1, looking southwest.

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Arizona Distributing Company Warehouse

Continuation Sheet No.7



View of the east elevation of ML-1, looking northwest.



View of the west elevation of the west addition to ML-1, looking east.

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Arizona Distributing Company Warehouse

Continuation Sheet No.8



Oblique view of the west and south elevations of the second west addition to ML-1, looking north-northeast.



View of the west and south elevations of Site ML-1, looking northeast.

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Arizona Distributing Company Warehouse

Continuation Sheet No.9



View of the rear (north) elevation of Site ML-1, looking west-southwest.



Aerial photograph of Site ML-1 (Google Earth, Imagery Date 5/23/2021)

STATE OF ARIZONA

HISTORIC PROPERTY INVENTORY FORM

Please type or print clearly. Fill out each applicable space accurately and with as much information as is known about the property. Use **continuation sheets where necessary**. Send completed form to: State Historic Preservation Office, 1100 W. Washington, Phoenix, AZ 85007

PROPERTY IDENTIFICATION

For properties identified through survey: Site No: ML-2 Survey Area: Mountain Line DCC

Historic Name(s): Viotti Furniture
(Enter the name(s), if any, that best reflects the property's historic importance.)

Address: 1 S. Milton Road (formerly 1 S. Sitgreaves Street)

City or Town: Flagstaff ☐ vicinity County: Coconino Tax Parcel No. 100-38-011

Township: 21N Range: 7E Section: 16 Quarter Section: SE1/4 Acreage: 0.14

Plat (Addition): Flagstaff Townsite Block: 2X Lot(s): 21-24 Year of plat (addition): 1890

Latitude: 35.197486 Longitude: -111.654353 USGS 7.5' quad map: Flagstaff West

Style: mid-century commercial box

Architect: _____ ☒ not determined ☐ known (source: _____)

Builder: _____ ☒ not determined ☐ known (source: _____)

Construction Date: 1956 ☐ known ☒ estimated (source: Coconino County Assessor)

STRUCTURAL CONDITION

☒ Good (well maintained, no serious problems apparent)

☐ Fair (some problems apparent) Describe: _____

☐ Poor (major problems; imminent threat) Describe: _____

☐ Ruin/Uninhabitable

USES/FUNCTIONS

Describe how the property has been used over time, beginning with the original use.
retail store

Sources: Arizona Daily Sun 1956,
1961, 1963, 1967, 1977, 1980

PHOTO INFORMATION

Date of photo: 12 August 2021
View Direction (looking towards)
west



SIGNIFICANCE

To be eligible for the National Register of Historic Places, a property must represent an important part of the history or architecture of an area. Note: a property need only be significant under one of the areas below to be eligible for the National Register.

- A. HISTORIC EVENTS/TRENDS (On a continuation sheet describe how the property is associated either with a significant historic event, or with a trend or pattern of events important to the history of the nation, the state, or a local community.)
- B. PERSON (On a continuation sheet describe how the property is associated with the life of a person significant in the past.)
- C. ARCHITECTURE (On a continuation sheet describe how the property embodies the distinctive characteristics of a type, period, or method of construction, or that represents the work of a master, or possesses high artistic values.)

Outbuildings: (Describe any other buildings or structures on the property and whether they may be considered historic.)

none

INTEGRITY

To be eligible for the National Register, a property must have integrity, that is, it must be able to visually convey its importance. Provide detailed information below about the property's integrity. Use continuation sheets if necessary.

1. LOCATION ☒ Original Site ☐ Moved (date _____) Original Site: _____
2. DESIGN (Describe alterations from the original design, including dates—known or estimated—when alterations were made)
northern section of the roof parapet replaced; composite wood panels façade and north and south elevations installed;
storefronts divided with the non-historic wood pilasters; and door on the façade replaced – dates undetermined
3. SETTING (Describe the natural and/or built environment around the property): The building is along a major
thoroughfare (S. Milton Road / U.S. Highway 89A) within the city of Flagstaff, and the surrounding area
includes the BNSF railroad and downtown Flagstaff to the north, single- and multi-family residential properties to
the west, and commercial and multi-family residential properties to the east and south.
- Describe how the setting has changed since the property's period of significance: Although the building's setting has
not changed substantially since its construction in 1956, the construction of a 5-story apartment building on the south
side of West Phoenix Avenue in 2016 changed the building's east-facing viewshed and diminished its integrity of
setting.
4. MATERIALS (Describe the materials used in the following elements of the property)
Walls (structure): concrete block Foundation: concrete Roof: composition roll
Windows: aluminum fixed storefront and glass block
If the windows have been altered, what were they originally? N/A
Wall Sheathing: painted structural concrete block; composite wood siding
If the sheathing has been altered, what was it originally? composite wood siding applied on top of structure
concrete block
5. WORKMANSHIP (Describe the distinctive elements, if any, of craftsmanship or method of construction)
The building is an unremarkable example of a mid-century commercial box, a modest vernacular building type
widely constructed in the U.S. after World War II. Modifications to the building since its construction have
compromised its historic integrity.

NATIONAL REGISTER STATUS (if listed, check the appropriate box)

☐ Individually listed; ☐ Contributor ☐ Noncontributor to _____ Historic District
Date Listed: _____ Determined eligible by Keeper of National Register (date: _____)

RECOMMENDATIONS OF ELIGIBILITY (opinion of SHPO staff or survey consultant)

Property ☐ is ☒ is not eligible individually.

Property ☐ is ☒ is not eligible as a contributor to a potential historic district.

☐ More information needed to evaluate.

If not considered eligible, state reason: Compromised integrity and lack of significance.

FORM COMPLETED BY

Name and Affiliation: Kirsten Johnson, AECOM

Mailing Address: 7720 N. 16th Street, Ste. 100, Phoenix, Arizona 85020

Date: 19 August 2021

Phone No.: (602) 861-7413

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Viotti Furniture

Continuation Sheet No.1

Site ML-2 is on a 0.14-acre parcel on the southwest corner of S. Milton Road and W. Phoenix Avenue in Flagstaff. Historic and modern aerial photographs show the site contains one commercial historic resource. Coconino County Assessor's data indicates a construction year of 1956. The building is visible on a 1964 aerial photograph, and historical newspaper articles indicate the building was occupied by May 1957 (*Arizona Daily Sun* 1957, 1963). Therefore, the 1956 year of construction appears to be accurate.

ML-2 is a one-story, reinforced concrete commercial building with a rectangular plan and a concrete foundation. The shed roof features a flat parapet wall on the façade and stepped parapets on the side elevations, and is clad with asphalt composition roll. The exterior walls are primarily painted, exposed, structural concrete block. Windows include fixed, aluminum-storefront units with concrete block sills on the façade, an aluminum-framed tripartite unit on the north elevation, and glass block units on the rear (west) elevation.

The building faces east toward S. Milton Road, and the façade features two angled storefronts, visually defined by non-historic wood pilasters applied to the corners of the building and between the storefronts. The larger, southern storefront includes a central bay containing a set of paired storefront windows and a non-historic, double-leaf metal glazed entry with a transom light. The entrance is recessed within the façade and northern window is installed at an angle. The central bay is flanked by ribbons of three fixed storefront windows; the southern set of windows features an exterior, tinted screen. All three bays on the façade are shaded by a non-historic, metal-framed, fabric awning. The smaller, northern storefront features an inset, single-entry aluminum and glass storefront door with a transom light bordered on the north by a set of paired storefront windows. The southern window is installed at an angle. The original flat parapet wall on the smaller storefront was replaced with an arched parapet after the historic period, and horizontal composite wood panels designed to simulate wood drop (shiplap) siding was installed over the structural concrete block. Non-historic, metal gooseneck sconces and signage also have been added to the northern storefront.

The south elevation lacks fenestration. A non-historic panel clad with composite wood has been installed on the east end. The north elevation includes an aluminum-framed tripartite window composed of a central fixed window flanked by siding windows. The non-historic panel clad with composite wood similar to the panel on the east elevation has been installed above the window. Wooden latticework has been attached to the exterior wall of the north elevation to encourage climbing plants. ML-2 was built into a naturally occurring hillside, the rear (west) elevation is only partially exposed. The exposed sections features a glass block window, two metal louvered vents, and three exposed metal beams. A paved parking lot is located adjacent to the rear elevation.

ML-2 was constructed in 1956 as Viotti Furniture. The business was initially founded in 1948 as Ragle's Used Furniture Store, which was located within a building south of ML-2 at 15 S. Sitgreaves Street (*Arizona Daily Sun* 1948). (ML-2's former address was 1 S. Sitgreaves Street; the section of S. Sitgreaves Street south of the BNSF

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Viotti Furniture

Continuation Sheet No.2

tracks was renamed S. Milton Road in the 1990s.) In 1949, Peter (Pete) Viotti purchased the business from the Ragle family (*Arizona Daily Sun* 1949). Mr. Viotti was born in Jerome, Arizona, in 1921, and after returning from service in World War II, he moved to Flagstaff. In addition to operating his furniture business, Pete also served on the Flagstaff City Council from 1960 to 1966 (*Verde Valley Independent & Camp Verde Bugle* 2011). Viotti continued to operate the business under the Ragle name until 1953, when he changed the name of the business to Viotti Furniture (*Arizona Daily Sun* 1953). Construction of ML-2 began in 1956, and in 1957, Viotti Furniture moved to its new location at 1 S. Sitgreaves Street (*Arizona Daily Sun* 1957). In 1961, the main store moved to 104 W. Birch Avenue, and the building at 1 S. Sitgreaves became Viotti's Warehouse Sales, which featured furniture at a reduced price (*Arizona Daily Sun* 1961, 1963). Viotti moved his warehouse sales out of the building at 1 S. Sitgreaves Street ca. 1966, and in 1967 the building was occupied by Harper's Used Furniture (*Arizona Daily Sun* 1967). In 1970, Harper's Furniture Company shared by space with Quick Clean Dry Cleaning (Flagstaff Directory Company 1970). Harper's Furniture Company was renamed National Liquidators in 1972 (*Arizona Daily Sun* 1972). In 1977, the building was occupied by Sun Chief Indian Jewelers, and by Rich Bros. Home Decorating Center in 1980 (*Arizona Daily Sun* 1977, 1980). The building is currently occupied by the Flagstaff Design Center.

ML-2 has not been moved and retains integrity of location. The building is along a major thoroughfare (S. Milton Road / U.S. Highway 89A) within the city of Flagstaff, and the surrounding area includes the BNSF railroad and downtown Flagstaff to the north, single- and multi-family residential properties to the west, and commercial and multi-family residential properties to the east and south. Although the building's setting has not changed substantially since its construction in 1956, the construction of a 5-story apartment building on the south side of West Phoenix Avenue in 2016 changed the building's east-facing viewshed and diminished its integrity of setting. The replacement of the northern section of the roof parapet, the installation of the composite wood panels on the façade and north and south elevations, the division of the storefronts with the non-historic wood pilasters, and the replacement door on the façade have compromised the building's integrity of design, materials, workmanship, feeling, and association.

Although Site ML-2 is associated with the commercial development of Flagstaff in the 1950s, historic research did not identify any associations with historical events or persons in history that rise to the level of significance needed to qualify for eligibility under NRHP Criterion A or B. ML-2 is an unremarkable example of a mid-century commercial box, a modest vernacular building type widely constructed in the U.S. after World War II. Modifications to the building since its construction have compromised its historic integrity and the building does not qualify for eligibility under NRHP Criterion C. Furthermore, the resource is not likely to yield information important to history or prehistory, and does not qualify for NRHP eligibility under Criterion D. Therefore, Site ML-2 is recommended not eligible for listing in the NRHP.

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Viotti Furniture

Continuation Sheet No.3

REFERENCES

Arizona Daily Sun. 1948. Advertisement for Ragle's Used Furniture. 13 April, p. 2.

_____. 1949. South Sitgreaves Store Changes Hands. 21 June, p. 2.

_____. 1953. Advertisement for the Viotti Furniture Company. 31 July, p. 5.

_____. 1956. Advertisement for Viotti Furniture. 1 May, p. 8.

_____. 1957. Advertisement for Viotti Furniture. 24 May, p. 3.

_____. 1959. Advertisement for Viotti Furniture. 27 March, p. 146.

_____. 1961. Advertisement for Warehouse Sales. 9 August, p. 8.

_____. 1963. Advertisement for Viotti Furniture. 17 August, p. 49.

_____. 1967. Advertisement for Harper's Used Furniture. 16 March, p. 13.

_____. 1972. Advertisement for National Liquidators. 24 May, p. 4.

_____. 1977. Wayne, Lee Parent Are New Sun Chief Owners. 4 December, p. 13.

_____. 1980. Advertisement for Rich Bros. 1 August, p. 9.

Flagstaff Directory Company. 1970. *Flagstaff City Directory including Oak Creek Canyon, Sedona, Bellemont, Parks, Grand Canyon, and Williams*. Flagstaff, Arizona.

Verde Valley Independent & Camp Verde Bugle. 2011. Obituary for Peter Viotti. Electronic document, <https://www.verdenews.com/news/2011/oct/05/peter-viotti-1921-2011/>, accessed August 2021.

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Viotti Furniture

Continuation Sheet No.4

PHOTOGRAPHS



Façade view of ML-2 in 1959, looking north-northwest (*Arizona Daily Sun* 1959).



Oblique view of the south elevation and façade of ML-2, looking north-northwest.

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Viotti Furniture

Continuation Sheet No.5



Detail of the façade of ML-2, looking south-southwest.



Oblique view of the façade and north elevation of ML-2, looking southwest.

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Viotti Furniture

Continuation Sheet No.6



Oblique view of the north and rear (west) elevation of ML-2, looking southeast.



Aerial photograph of Site ML-2 (Google Earth, Imagery Date 5/23/2021)

STATE OF ARIZONA

HISTORIC PROPERTY INVENTORY FORM

Please type or print clearly. Fill out each applicable space accurately and with as much information as is known about the property. Use continuation sheets where necessary. Send completed form to: State Historic Preservation Office, 1100 W. Washington, Phoenix, AZ 85007

PROPERTY IDENTIFICATION

For properties identified through survey: Site No: ML-3 Survey Area: Mountain Line DCC

Historic Name(s): Standard Stations, Inc.
(Enter the name(s), if any, that best reflects the property's historic importance.)

Address: 6 S. Milton Road (formerly 6 S. Sitgreaves Street)

City or Town: Flagstaff ☐ vicinity County: Coconino Tax Parcel No. 100-39-020B

Township: 21N Range: 7E Section: 16 Quarter Section: SE1/4 Acreage: 0.31

Plat (Addition): Flagstaff Townsite Block: 1X Lot(s): 6-12 Year of plat (addition): 1890

Latitude: 35.197181 Longitude: -111.653967 USGS 7.5' quad map: Flagstaff West

Style: post-World War II box station / oblong box with canopy

Architect: _____ ☒ not determined ☐ known (source: _____)

Builder: _____ ☒ not determined ☐ known (source: _____)

Construction Date: 1960 ☐ known ☒ estimated (source: Arizona Daily Sun 1960)

STRUCTURAL CONDITION

☒ Good (well maintained, no serious problems apparent)

☐ Fair (some problems apparent) Describe: _____

☐ Poor (major problems; imminent threat) Describe: _____

☐ Ruin/Uninhabitable

USES/FUNCTIONS

Describe how the property has been used over time, beginning with the original use.
gas station; auto repair

Sources: Arizona Daily Sun 1960,
historic telephone directories

PHOTO INFORMATION

Date of photo: 12 August 2021
View Direction (looking towards)
northeast



SIGNIFICANCE

To be eligible for the National Register of Historic Places, a property must represent an important part of the history or architecture of an area. Note: a property need only be significant under one of the areas below to be eligible for the National Register.

- A. HISTORIC EVENTS/TRENDS (On a continuation sheet describe how the property is associated either with a significant historic event, or with a trend or pattern of events important to the history of the nation, the state, or a local community.)
- B. PERSON (On a continuation sheet describe how the property is associated with the life of a person significant in the past.)
- C. ARCHITECTURE (On a continuation sheet describe how the property embodies the distinctive characteristics of a type, period, or method of construction, or that represents the work of a master, or possesses high artistic values.)

Outbuildings: (Describe any other buildings or structures on the property and whether they may be considered historic.)

none

INTEGRITY

To be eligible for the National Register, a property must have integrity, that is, it must be able to visually convey its importance. Provide detailed information below about the property's integrity. Use continuation sheets if necessary.

1. LOCATION ☒ Original Site ☐ Moved (date _____) Original Site: _____
2. DESIGN (Describe alterations from the original design, including dates—known or estimated—when alterations were made)
replacement or alteration of the attached canopy, installation of non-historic overhanging roof parapets, infill of clerestory windows, installation of shorter windows and an additional entrance on the southern section of the façade, replacement roll-up doors on the south elevation, and construction of rear additions— dates undetermined
3. SETTING (Describe the natural and/or built environment around the property): The building is along a major thoroughfare (S. Milton Road / U.S. Highway 89A) within the city of Flagstaff, and the surrounding area includes the BNSF railroad and downtown Flagstaff to the north, single- and multi-family residential and commercial to the west, east, and south.
Describe how the setting has changed since the property's period of significance: Although the building's setting has not changed substantially since its construction ca. 1960, the construction of a 5-story apartment building behind Site ML-3 on the south side of W. Phoenix Avenue in 2016 changed its historic backdrop and diminished its integrity of setting.
4. MATERIALS (Describe the materials used in the following elements of the property)
Walls (structure): metal frame Foundation: concrete Roof: built-up; asphalt shingles
Windows: aluminum fixed storefront
If the windows have been altered, what were they originally? N/A
Wall Sheathing: prefabricated metal panels
If the sheathing has been altered, what was it originally? N/A
5. WORKMANSHIP (Describe the distinctive elements, if any, of craftsmanship or method of construction)
ML-3 is an example of a 1960s-era Standard Oil Company (California) service station. It was one of many constructed using a standardized design, and because of its compromised integrity it is no longer representative of this design.

NATIONAL REGISTER STATUS (if listed, check the appropriate box)

☐ Individually listed; ☐ Contributor ☐ Noncontributor to _____ Historic District
Date Listed: _____ Determined eligible by Keeper of National Register (date: _____)

RECOMMENDATIONS OF ELIGIBILITY (opinion of SHPO staff or survey consultant)

Property ☐ is ☒ is not eligible individually.
Property ☐ is ☒ is not eligible as a contributor to a potential historic district.
☐ More information needed to evaluate.
If not considered eligible, state reason: Compromised integrity.

FORM COMPLETED BY

Name and Affiliation: Kirsten Johnson, AECOM
Mailing Address: 7720 N. 16th Street, Ste. 100, Phoenix, Arizona 85020

Date: 24 August 2021
Phone No.: (602) 861-7413

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Standard Stations, Inc.

Continuation Sheet No.1

Site ML-3 is on a 0.31-acre parcel on the east side of S. Milton Road, south of W. Phoenix Avenue. Historic and modern aerial photographs show the site contains one commercial historic resource. Coconino County Assessor's date indicates a construction year of 1965. The 1948 Flagstaff Sanborn Fire Insurance map depicts a gas and oil station on the site of ML-3, which was connected to a grocery store on the parcel to the north. A historic newspaper article indicates the building was constructed as a combination service station and grocery store in 1938 (*Arizona Daily Sun* 1948). Historic aerial photographs indicate the section of the building connecting the store and service station buildings had been demolished by 1954, but the store and service station remained extant as separate buildings. Aerial photographs suggest that the 1938 service station building was demolished and replaced with the current structure, ML-3 between 1955 and 1964. Between 1959 and 1960, the site of ML-3 was transferred to Standard Stations, Inc. (*Arizona Daily Sun* 1960), and ML-3's original design is consistent with the standardized plan used for filling stations operated by Standard Oil in the 1960s. Therefore, the 1965 year of construction included in the county assessor data appears to be slightly inaccurate, and the estimated year of construction for Site ML-3 is ca. 1960.

ML-3 is a one-story, metal-framed commercial building with a concrete foundation and a roughly rectangular plan composed of the original footprint and a rear addition with an attached open storage area. The original section of the building and the first rear addition feature flat roofs with parapets that appear to be clad with built-up material. The second addition, which is constructed adjacent to the north elevation of the first rear addition, features a shed roof clad with asphalt composition shingles. The exterior walls are clad with prefabricated metal panels. Windows include fixed storefront and casement units.

The building faces west toward S. Milton Road, and its façade features a ribbon of three storefront windows; a single-entry metal flush door with a fixed light and an infilled transom; a set of paired storefront windows; a single-entry flush metal door with a fixed light, an aluminum kickplate, and a transom light; and a ribbon of four storefront windows. This fenestration is bordered on the top by a ribbon of fixed clerestory windows that run the full width of the façade. The clerestories on the north end of the façade have been painted over or infilled. The façade also includes an overhanging parapet and an attached canopy that originally shaded gas pumps that are no longer extant. The canopy features metal pole supports and a metal roof with an overhanging parapet similar to the parapet on the building's façade. Historic aerial photographs, images of the 1960s Standard Station's standardized design, and patterns in the existing pavement in front of ML-3 suggest that the canopy has either been replaced or had been altered by removing an additional bay from the west end and modifying the original parapet.

The north elevation of the original section of the building also features the non-historic overhanging roof parapet. Three painted or infilled clerestory windows and a ribbon of three storefront windows are located on the west end of the elevation. The center and east end of the north elevation include two casement windows with textured glass

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Standard Stations, Inc.

Continuation Sheet No.2

above two single-entry metal flush doors, which provided access to the filling station's public restrooms. On the south elevation, the non-historic parapet and a ribbon of clerestory windows are, along with two vehicle bays with aluminum roll-up doors flanked by fixed storefront windows.

The rear (east) elevation includes a partial-width addition with an attached, shed-roofed, open storage area. The addition is not visible on a 1964 aerial photograph, but likely was constructed during the historic period defined for this study (prior to 1981). Its south elevation exhibits the same overhanging parapet present on the original section of the building and two vehicle bays with aluminum roll-up doors. The addition's east and north elevations feature flush parapets. The east elevation also includes a ribbon of storefront windows. Views of the north elevation are concealed by the shed-roofed storage area, which is enclosed with a chain-link fence with privacy slats.

ML-3 appears to have been constructed ca. 1960 as a Standard filling station (*Arizona Daily Sun* 1960). (The original address of ML-3 was 6 S. Sitgreaves Street; the section of S. Sitgreaves Street south of the BNSF tracks was renamed S. Milton Road in the 1990s.) The Standard Oil Company (California) was established in 1906 with the consolidation of the Pacific Coast Oil and Iowa Standard. By the end of 1919, the company had 218 service stations within its network. Development of highway system in the 1920s and 1930s led to the establishment of Standard Service Stations, and by 1926, the company had 735 stations in five-state marketing area. In 1930, the Standard Oil Company began using the name "Chevron" on its products (Chevron 2021). In the 1940s, Chevron Stations that were owned or leased by individuals and sold Chevron gasoline emerged, although the Standard Oil Company continued to own and operate its own Standard Stations. Prior to 1960, the station at 6 S. Milton Road appears to have been operated as a owner-operated Chevron Station, first as the Caffey Brothers Chevron Station (*Arizona Daily Sun* 1946) and then the Jim Gale Chevron Station (*Arizona Daily Sun* 1951; Flagstaff Directory Company 1959). Historical maps and aerial photographs suggest that the original station was removed and replaced with ML-3 ca. 1960, and the local newspapers and directories indicate it was operated as a Standard Station by the Standard Oil Company throughout the 1960s. Company-owned stations began to be phased out in the late 1960s and 1970s, and in 1969, ML-3 was occupied by Chuck Upton's Chevron Station (*Arizona Daily Sun* 1969). The building is currently occupied by Brake Masters.

ML-3 has not been moved and retains integrity of location. The building is along a major thoroughfare (S. Milton Road / U.S. Highway 89A) within the city of Flagstaff, and the surrounding area includes the BNSF railroad and downtown Flagstaff to the north, single- and multi-family residential and commercial to the west, east, and south. Although the building's setting has not changed substantially since its construction ca. 1960, the construction of a 5-story apartment building behind Site ML-3 on the south side of W. Phoenix Avenue in 2016 changed its historic backdrop and diminished its integrity of setting. The 1960s standardized station design for the Standard Oil Company (California) featured an oblong box with fixed storefront and ribbons of clerestory windows, a flat

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Standard Stations, Inc.

Continuation Sheet No.3

roof with a low parapet, and an attached, flat-roofed canopy. Although Site ML-3 retains its clerestory windows and storefront window groupings and can be identified as an example of the 1960s standardized design, modifications to the building since its construction have compromised its integrity of design, materials, workmanship, feeling, and association and the building is no longer a representative example of this type. Modifications include the replacement or alteration of the attached canopy, the non-historic overhanging roof parapets, infill of clerestory windows, installation of shorter windows and an additional entrance on the southern section of the façade, replacement roll-up doors on the south elevation, and the rear additions.

Although Site ML-3 is associated with the commercial development of Flagstaff and roadside businesses along U.S. Highway 89A in the 1960s, the building lacks integrity and no longer conveys association with significant historical events or themes and does not qualify for eligibility under NRHP Criterion A. Historic research did not identify any associations with significant persons in history, and the building does not qualify for eligibility under NRHP Criterion B. ML-3 is an example of a 1960s-era Standard Oil Company (California) service station. It was one of many constructed using a standardized design, and because of its compromised integrity, it is no longer representative of this design and is does not qualify for eligibility under NRHP Criterion C. Furthermore, the building is not likely to yield information important to history or prehistory, and does not qualify for NRHP eligibility under Criterion D. Therefore, Site ML-3 is recommended not eligible for listing in the NRHP.

REFERENCES

Arizona Daily Sun. 1946. Advertisement for Standard Stations Inc., and Chevron Gas Stations. 16 October, p. 4.

_____. 1948. Pathways of Old Memories. 25 March, p. 4.

_____. 1951. Advertisement for the Jim Gale Chevron Station. 24 July, p. 7.

_____. 1960. Classified Ad for Standard Stations, Inc. 23 April, p. 3.

_____. 1969. Chamber Gets Boost in Members. 27 October, p. 2.

Chevron. 2021. History. Electronic document: <https://www.chevron.com/about/history>, accessed August 2021.

Flagstaff Directory Company. 1959. *Flagstaff City Directory including Oak Creek Canyon, Sedona, Bellemont, Parks, Grand Canyon, and Williams*. Flagstaff, Arizona.

Fresno Bee. 2021. Discerning between Standard and Chevron gas stations. Electronic document:

<https://www.fresnobee.com/opinion/letters-to-the-editor/article47242845.html>, accessed August 2021.

www.gassigns.org. 2021. Standard of California/Chevron photos. Electronic document: www.gassigns.org/chevron.htm, accessed August 2021.

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Standard Stations, Inc.

Continuation Sheet No.4

PHOTOGRAPHS



Standard Oil Company (California) Service Station in the 1960s (Source: www.gassigns.org).



Oblique view of the façade and south elevation of ML-3, looking north-northeast.

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Standard Stations, Inc.

Continuation Sheet No.5



Oblique view of the north elevation and façade of ML-3, looking southeast.



Oblique view of the rear (east) and north elevations of ML-3, looking southwest.

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Standard Stations, Inc.

Continuation Sheet No.6



View of the south elevation of ML-3, looking northwest.



Aerial photograph of Site ML-3 (Google Earth, Imagery Date 5/23/2021)

STATE OF ARIZONA

HISTORIC PROPERTY INVENTORY FORM

Please type or print clearly. Fill out each applicable space accurately and with as much information as is known about the property. Use continuation sheets where necessary. Send completed form to: State Historic Preservation Office, 1100 W. Washington, Phoenix, AZ 85007

PROPERTY IDENTIFICATION

For properties identified through survey: Site No: ML-4 Survey Area: Mountain Line DCC

Historic Name(s): Wheeler's Underpass Grocery
(Enter the name(s), if any, that best reflects the property's historic importance.)

Address: 2 S. Milton Road (formerly 2 S. Sitgreaves Street)

City or Town: Flagstaff ☐ vicinity County: Coconino Tax Parcel No. 100-39-020A

Township: 21N Range: 7E Section: 16 Quarter Section: SE1/4 Acreage: 0.19

Plat (Addition): Flagstaff Townsite Block: 1X Lot(s): 6-12 Year of plat (addition): 1890

Latitude: 35.19735 Longitude: -111.653814 USGS 7.5' quad map: Flagstaff West

Style: Commercial

Architect: _____ ☒ not determined ☐ known (source: _____)

Builder: W.C. McCudden ☐ not determined ☒ known (source: Arizona Daily Sun 1948a)

Construction Date: 1938 ☒ known ☐ estimated (source: Arizona Daily Sun 1948a)

STRUCTURAL CONDITION

☒ Good (well maintained, no serious problems apparent)

☐ Fair (some problems apparent) Describe: _____

☐ Poor (major problems; imminent threat) Describe: _____

☐ Ruin/Uninhabitable

USES/FUNCTIONS

Describe how the property has been used over time, beginning with the original use.
grocery store; package store;
sporting goods store

Sources: Arizona Daily Sun 1948a,
1950; Flagstaff telephone directories

PHOTO INFORMATION

Date of photo: 12 August 2021
View Direction (looking towards)
east



SIGNIFICANCE

To be eligible for the National Register of Historic Places, a property must represent an important part of the history or architecture of an area. Note: a property need only be significant under one of the areas below to be eligible for the National Register.

- A. HISTORIC EVENTS/TRENDS (On a continuation sheet describe how the property is associated either with a significant historic event, or with a trend or pattern of events important to the history of the nation, the state, or a local community.)
- B. PERSON (On a continuation sheet describe how the property is associated with the life of a person significant in the past.)
- C. ARCHITECTURE (On a continuation sheet describe how the property embodies the distinctive characteristics of a type, period, or method of construction, or that represents the work of a master, or possesses high artistic values.)

Outbuildings: (Describe any other buildings or structures on the property and whether they may be considered historic.)
none

INTEGRITY

To be eligible for the National Register, a property must have integrity, that is, it must be able to visually convey its importance. Provide detailed information below about the property's integrity. Use continuation sheets if necessary.

1. LOCATION ☒ Original Site ☐ Moved (date _____) Original Site: _____
2. DESIGN (Describe alterations from the original design, including dates—known or estimated—when alterations were made)
modifications to the building's storefront, including replacement windows, a replacement door, an altered or modified awning, and the application of the stone veneer— dates undetermined
3. SETTING (Describe the natural and/or built environment around the property): The building is along a major thoroughfare (S. Milton Road / U.S. Highway 89A) within the city of Flagstaff, and the surrounding area includes the BNSF railroad and downtown Flagstaff to the north, single- and multi-family residential and commercial to the west, east, and south.

Describe how the setting has changed since the property's period of significance: Although the building's setting has not changed substantially since its construction ca. 1960, the construction of a 5-story apartment building behind Site ML-4 on the south side of West Phoenix Avenue in 2016 changed its historic backdrop and diminished its integrity of setting.
4. MATERIALS (Describe the materials used in the following elements of the property)
Walls (structure): masonry Foundation: concrete Roof: built-up; asphalt shingles
Windows: aluminum fixed storefront; two-part aluminum sliding; steel casement and awning
If the windows have been altered, what were they originally? fixed storefront
Wall Sheathing: painted brick; stone veneer
If the sheathing has been altered, what was it originally? possibly unpainted brick
5. WORKMANSHIP (Describe the distinctive elements, if any, of craftsmanship or method of construction)
ML-4 is an example of a modest commercial box.

NATIONAL REGISTER STATUS (if listed, check the appropriate box)

☐ Individually listed; ☐ Contributor ☐ Noncontributor to _____ Historic District
Date Listed: _____ Determined eligible by Keeper of National Register (date: _____)

RECOMMENDATIONS OF ELIGIBILITY (opinion of SHPO staff or survey consultant)

Property ☐ is ☒ is not eligible individually.

Property ☐ is ☒ is not eligible as a contributor to a potential historic district.

☐ More information needed to evaluate.

If not considered eligible, state reason: Compromised integrity, lack of significance.

FORM COMPLETED BY

Name and Affiliation: Kirsten Johnson, AECOM

Mailing Address: 7720 N. 16th Street, Ste. 100, Phoenix, Arizona 85020

Date: 24 August 2021

Phone No.: (602) 861-7413

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Wheeler's Underpass Grocery

Continuation Sheet No.1

Site ML-4 is on a 0.19-acre parcel on the southeast corner of S. Milton Road and W. Phoenix Avenue. Historic and modern aerial photographs show the site contains one commercial historic resource. Coconino County Assessor's data indicates a construction year of 1967. A historic newspaper article indicates the building was constructed as a combination service station and grocery store in 1938, which is depicted on the 1948 Flagstaff Sanborn Fire Insurance map (*Arizona Daily Sun* 1948a). A historic aerial photograph indicates that the section of the building connecting the store and service station buildings had been demolished by 1954, but the store and service station remained as separate buildings. The service station appears to have been demolished and replaced by a new service station ca. 1960 (Site ML-3), but the store building, which is designated ML-4, remains extant. Based on this evidence, as well as the building's scale and design, the 1938 year of construction for ML-4 is considered accurate.

ML-4 is a one-story, masonry commercial building with a concrete foundation and an L-shaped plan composed of the original main block and a rear addition that was constructed during the historic period defined for this study (prior to 1981). The roof is flat with parapets on the façade and side elevations, and features an overhang in the rear elevation. The roof appears to be clad with built-up material and the exterior walls are brick with a section of stone veneer on the façade. Windows include aluminum-framed fixed storefront units, two-part aluminum sliding units, and steel-framed casement and awning units.

The building faces west toward S. Milton Road. The storefront includes a corner entry on the building's northwest corner, which is flanked by a fixed storefront window on the north elevation and a ribbon of three fixed storefront windows on the façade. The entrance is a single-entry aluminum and glass storefront door with a transom light. The full width of the façade, the corner entry, and the storefront window on the north elevation are shaded by a wood-framed awning clad with asphalt composition shingles. Non-historic stone veneer has been applied on the storefront's bulkheads, and a modern light box sign is mounted to the façade parapet. The storefront windows and the entrance's transom light feature interior wrought iron security bars, and the door is protected by one vertical and five horizontal metal bars mounted on its interior side. Roll-up security screens also have been mounted above the storefront.

The south elevation of the original section of the building includes a two-part aluminum sliding window and a four-light steel casement window, and the south elevation of the rear addition lacks fenestration and architectural detail. The rear (east) elevation of the addition features a small window that appears to be a steel-framed awning unit with a hinged wooden screen. The north elevation of the original section of the building has three, small, infilled window openings with brick sills. The rear addition is wider than the original section of the building on the north side, and its north elevation features angled corners, a small window similar to the window on the rear (east) elevation, and a single-entry metal flush door.

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Wheeler's Underpass Grocery

Continuation Sheet No.2

Historical research suggests that ML-4 was constructed in 1938 a combination service station and grocery store (*Arizona Daily Sun* 1948). (The original address of ML-4 was 2 S. Sitgreaves Street; the section of S. Sitgreaves Street south of the BNSF tracks was renamed S. Milton Road in the 1990s.) The Webber Brothers contracted builder W.C. McCudden to construct the building, which was originally occupied by Wheeler's Underpass Grocery, operated by Gordon Wheeler (*Arizona Daily Sun* 1946, 1948a). Mr. Wheeler subsequently diversified his business by adding sporting goods to his inventory, and in 1950, the business was known as Wheeler's Grocery and Sporting Goods Store (*Arizona Daily Sun* 1950). After the death of Mr. Wheeler, his widow sold the building and all of its merchandise to Jerome K. and Lucille Ruff in August 1953 (*Arizona Daily Sun* 1953). In September 1953, Mr. Ruff announced the relocation of his existing business, known as Ruff's Package Store, to the building at 2 S. Sitgreaves Street (*Arizona Daily Sun* 1953b). Prior to the move, Ruff's Package Store was located at 8 N. San Francisco Street in downtown Flagstaff and Mr. Ruff's business appears to have been limited to liquor sales (*Arizona Daily Sun* 1948b). With the purchase of Wheeler's property, Mr. Ruff expanded his business to include sporting goods and groceries. Flagstaff telephone directories indicate the business retained the Ruff's Package Store name throughout the 1950s, but in the 1960s the name was changed to Ruff's Liquor and Sporting Goods. The business has continuously occupied the building since 1953, as the building is currently occupied by Ruff's Sporting Goods, which appears to specialize in gun sales.

ML-4 has not been moved and retains integrity of location. The building is along a major thoroughfare (S. Milton Road / U.S. Highway 89A) within the city of Flagstaff, and the surrounding area includes the BNSF railroad and downtown Flagstaff to the north, single- and multi-family residential and commercial to the west, east, and south. Although the building's setting has not changed substantially since its construction in 1938, the construction of a 5-story apartment building behind Site ML-4 on the south side of W. Phoenix Avenue in 2016 changed its historic backdrop and diminished its integrity of setting. Modifications to the building's storefront, including replacement windows, a replacement door, an altered or modified awning, and the application of the stone veneer, have compromised the building's integrity of design, materials, workmanship, feeling, and association.

Although Site ML-4 is associated with the commercial development of Flagstaff, it does not convey association with significant historical events or themes and does not qualify for eligibility under NRHP Criterion A. Historic research did not identify any associations with significant persons in history, and the building does not qualify for eligibility under NRHP Criterion B. ML-4 is an example of a modest commercial box and it does not possess sufficient architectural significance to qualify for eligibility under NRHP Criterion C. Furthermore, the building is not likely to yield information important to history or prehistory, and does not qualify for NRHP eligibility under Criterion D. Therefore, Site ML-4 is recommended not eligible for listing in the NRHP.

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Wheeler's Underpass Grocery

Continuation Sheet No.3

REFERENCES

Arizona Daily Sun. 1946. Personals. 30 December, p. 3.

_____. 1948a. Pathways of Old Memories. 25 March, p. 4.

_____. 1948b. Advertisement for Ruff's Package Store. 21 December, p. 7.

_____. 1950. Advertisement for Wheeler's Grocery and Sporting Goods Store. 20 April, p. 12.

_____. 1953a. Legal Notice. 20 August, p. 9.

_____. 1953b. Advertisement for Ruff's Package Store. 4 September, p. 7.

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Wheeler's Underpass Grocery

Continuation Sheet No.4

PHOTOGRAPHS



Oblique view of the north elevation and façade of Site ML-4, looking southeast.



Oblique view of the façade and south elevation of ML-4, looking northeast.

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Wheeler's Underpass Grocery

Continuation Sheet No.5



Oblique view of the south and rear (east) elevations of ML-4, looking west-northwest.



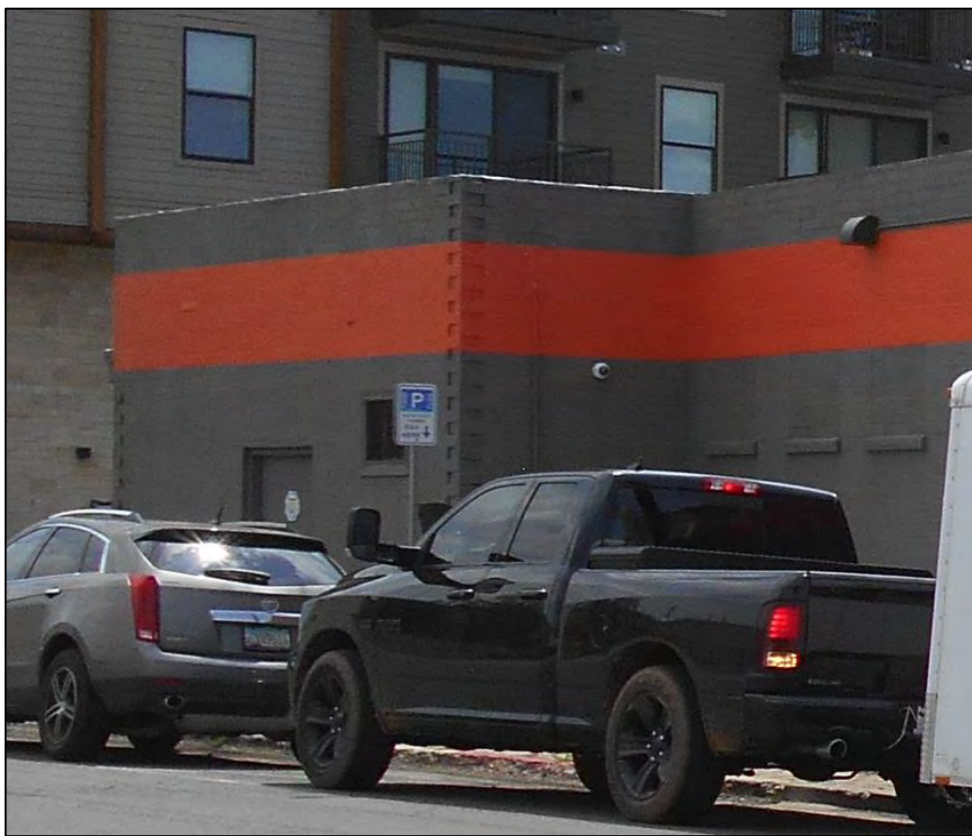
Oblique view of the rear (east) and north elevations of ML-4, looking southwest.

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Wheeler's Underpass Grocery

Continuation Sheet No.6



Detail of the north elevation of ML-4, looking southeast.



Aerial photograph of Site ML-3 (Google Earth, Imagery Date 5/23/2021)

STATE OF ARIZONA

HISTORIC PROPERTY INVENTORY FORM

Please type or print clearly. Fill out each applicable space accurately and with as much information as is known about the property. Use continuation sheets where necessary. Send completed form to: State Historic Preservation Office, 1100 W. Washington, Phoenix, AZ 85007

PROPERTY IDENTIFICATION

For properties identified through survey: Site No: ML-5 Survey Area: Mountain Line DCC

Historic Name(s): Flagstaff Chamber of Commerce
(Enter the name(s), if any, that best reflects the property's historic importance.)

Address: 101 W. Historic Route 66 (101 W. Santa Fe)

City or Town: Flagstaff ☐ vicinity County: Coconino Tax Parcel No. 100-44-006B

Township: 21N Range: 7E Section: 16 Quarter Section: SE1/4 Acreage: 0.37

Plat (Addition): N/A Block: N/A Lot(s): N/A Year of plat (addition): N/A

Latitude: 35.19775 Longitude: -111.65065 USGS 7.5' quad map: Flagstaff West

Style: no style / vernacular

Architect: Lester Byron ☐ not determined ☒ known (source: Arizona Daily Sun 1952i)

Builder: Albert M. Klopping ☐ not determined ☒ known (source: Arizona Daily Sun 1952e)

Construction Date: 1952 ☒ known ☐ estimated (source: Arizona Daily Sun 1952c)

STRUCTURAL CONDITION

☒ Good (well maintained, no serious problems apparent)

☐ Fair (some problems apparent) Describe: _____

☐ Poor (major problems; imminent threat) Describe: _____

☐ Ruin/Uninhabitable

USES/FUNCTIONS

Describe how the property has been used over time, beginning with the original use.
offices – Flagstaff Chamber of Commerce

Sources: Arizona Daily Sun 1952i
Google Image Capture March 2021

PHOTO INFORMATION

Date of photo: March 2021
View Direction (looking towards) south



SIGNIFICANCE

To be eligible for the National Register of Historic Places, a property must represent an important part of the history or architecture of an area. Note: a property need only be significant under one of the areas below to be eligible for the National Register.

- A. HISTORIC EVENTS/TRENDS (On a continuation sheet describe how the property is associated either with a significant historic event, or with a trend or pattern of events important to the history of the nation, the state, or a local community.)
- B. PERSON (On a continuation sheet describe how the property is associated with the life of a person significant in the past.)
- C. ARCHITECTURE (On a continuation sheet describe how the property embodies the distinctive characteristics of a type, period, or method of construction, or that represents the work of a master, or possesses high artistic values.)

Outbuildings: (Describe any other buildings or structures on the property and whether they may be considered historic.)

none

INTEGRITY

To be eligible for the National Register, a property must have integrity, that is, it must be able to visually convey its importance. Provide detailed information below about the property's integrity. Use continuation sheets if necessary.

1. LOCATION ☒ Original Site ☐ Moved (date _____) Original Site: _____
2. DESIGN (Describe alterations from the original design, including dates—known or estimated—when alterations were made)
addition of second stories with side-gabled roofs to the original main block and the east projection, replacement of the porch's original flat roof had with a conical roof, increase of the height and changes to the angle of the parapet wall between the main block and the eastern projection, and construction of the western projection – 1980; installation of storefront system, replacement of primary entrance, and replacement windows – between 2016 and 2018.
3. SETTING (Describe the natural and/or built environment around the property): The building is along Historic Route 66 within the City of Flagstaff, and the surrounding area includes the BNSF Railroad to the south, downtown Flagstaff to the north, a vacant lot to the west, and a parking lot and the historic Santa Fe Passenger Depot to the east.
Describe how the setting has changed since the property's period of significance: The building's setting has not changed substantially since its construction.
4. MATERIALS (Describe the materials used in the following elements of the property)
Walls (structure): concrete block, wood Foundation: concrete Roof: asphalt shingles
Windows: fixed storefront; 1/1 vinyl sash; two-part vinyl sliding
If the windows have been altered, what were they originally? multi-light casement on facade
Wall Sheathing: painted brick; composite siding
If the sheathing has been altered, what was it originally? possibly unpainted brick
5. WORKMANSHIP (Describe the distinctive elements, if any, of craftsmanship or method of construction)
ML-5 is was constructed with elements of the Modern/Contemporary style. Major renovations completed in 1980 concealed these elements.

NATIONAL REGISTER STATUS (if listed, check the appropriate box)

☐ Individually listed; ☐ Contributor ☐ Noncontributor to _____ Historic District
Date Listed: _____ Determined eligible by Keeper of National Register (date: _____)

RECOMMENDATIONS OF ELIGIBILITY (opinion of SHPO staff or survey consultant)

Property ☐ is ☒ is not eligible individually.

Property ☐ is ☒ is not eligible as a contributor to a potential historic district.

☐ More information needed to evaluate.

If not considered eligible, state reason: Compromised integrity.

FORM COMPLETED BY

Name and Affiliation: Kirsten Johnson, AECOM

Mailing Address: 7720 N. 16th Street, Ste. 100, Phoenix, Arizona 85020

Date: 27 August 2021

Phone No.: (602) 861-7413

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Flagstaff Chamber of Commerce

Continuation Sheet No.1

Site ML-5 is on a 0.37-acre parcel on the southwest corner of W. Route 66 and S. Beaver Street. Historic and modern aerial photographs show the site contains one historic commercial resource. Coconino County Assessor's data indicates a construction year of 1970, but historical research has determined the 1970 year of construction to be inaccurate. A cornerstone installed within the original porch wall indicates a 1952 year of construction, and historic newspaper articles document groundbreaking for the building in June 1952 and the completion of the building in September 1952 (*Arizona Daily Sun* 1952c, 1952f, 1952g).

ML-5 is a two-story, concrete block and wood-framed vernacular office building with a concrete foundation and a roughly rectangular plan composed of a side-gabled main block with side-gabled projections on the east and west elevations that both feature shorter south-facing slopes, similar in appearance to a salt box roof. The original building was one story and built in 1952. A second story and the western projection were added to the original building in 1980. The building's roof features wide, overhanging eaves and are clad with asphalt composition shingles. The exterior walls of the original section of the building are exposed, structural, stacked bond, concrete block, and the exterior walls of the additions are clad with horizontal composite board siding.

The building faces north toward W. Route 66, and the façade of the main block features a non-historic aluminum and glass storefront system composed of a centered single-entry door with a transom light that is flanked by sets of paired, three-part fixed windows. The storefront system is shaded by a flat roofed metal awning supported by brackets. The façade of the projecting bay addition on the west elevation of the main block includes a single-entry metal flush door. A slanted, concrete block parapet wall is located adjacent to the east elevation of the main block and projects from the primary façade. A one-story bay is located east of the wall and features two, deeply recessed, wood-framed fixed windows with an adjacent brick planter. This bay is contained beneath the conical roof of a one-story porch attached to the building's east elevation. The porch features three, stacked bond support piers and the porch deck is constructed of concrete pavers.

On the east elevation, the porch shades the primary entrance, which is composed of a replacement wood double entry flanked on the north by a narrow, vertical fixed window and on the south by a full-height, northeast-facing bay window composed of three narrow, vertical, fixed lights. The east elevation of the eastern projection includes a ribbon of five, fixed, wood-framed windows with an adjacent planter in the lower level. Its upper level includes a set of paired 1/1 vinyl sash windows in the gable end and two stepped vinyl sash windows that probably light in interior staircase. The gable end of the main block lacks fenestration and architectural detail. The upper level of the projection and the main block both include composite siding designed to mimic wood shingles in their gable ends.

As on the east elevation, both the main block and the western projection both feature composite siding designed to mimic wood shingles in their gable ends. The projection is an addition and its exterior walls are clad with

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Flagstaff Chamber of Commerce

Continuation Sheet No.2

composite siding. The lower level of the projection features an individual 1/1 vinyl sash window. and the upper levels of both the projection and the main block both include three, stepped, 1/1 vinyl sash windows.

The rear (south) elevation of the western projection includes two 1/1 vinyl sash windows in its lower level and two 1/1 vinyl sash windows in its upper level. The rear (south) elevation of the main block includes two, 2-part, vinyl-framed sliding windows in its lower level, and its upper level features a set of paired sliding windows and a projecting bay with a ribbon of four sliding windows.

ML-5 was constructed in 1952 to house the offices of the Flagstaff Chamber of Commerce. Early in 1952, the Chamber of Commerce began raising \$20,000 to construct the new building through the sale of promissory notes. By late April, \$15,000 had been raised and by May 1952, the Chamber of Commerce had raised almost the full amount (*Arizona Daily Sun* 1952a, 1952b). Ground was officially broken for the new building on 23 June 1952, which was located on property leased to the Chamber of Commerce by the Santa Fe Railroad (*Arizona Daily Sun* 1952c). Phoenix architect Lester Byron completed the design for the building and Albert M. Klopping was the general contractor (*Arizona Daily Sun* 1952e, 1952i).

By the beginning of August 1952, the construction project was halfway complete (*Arizona Daily Sun* 1952d, 1952e), and the building was finished in September (*Arizona Daily Sun* 1952g). Local businesses assisted the Flagstaff-based general contractor with the trade work and supplied building materials. N.J. Shaum & Son furnished and installed the electrical work, paint was supplied by Wensell's Paint Store, PBSW provided office supplies, and the Harenburg Cinder Block Company provided the concrete blocks (*Arizona Daily Sun* 1952h).

In early October 1952, the Chamber of Commerce held an open house for the public to inspect the new building, which was described as "modern, but with a western air suited to its terrain" (*Arizona Daily Sun* 1952h). The original, one-story concrete block section of the building was Modern in design, with elements of the Contemporary style exhibited by widely overhanging eaves, a broad expanse of uninterrupted wall surface and clerestory windows on the façade, an asymmetrical design, stacked bond concrete block construction, and a recessed front entry (*Arizona Daily Sun* 1952a). Another modern element was a projecting bay with a slanted or shed roof attached to the east elevation of the flat-roofed main block. The projecting bay featured a lower roof profile than the main block, creating varied ceiling heights on the interior of the building. The two sections were visually separated by a rectangular parapet wall that projected from the façade (north).

In September 1979, plans were underway to expand and modernize ML-5, using building materials and labor donated by local businesses (*Arizona Daily Sun* 1979a, 1979b). The architect for the renovation project was Bert Bender (*Arizona Daily Sun* 1979a). When the project was complete in the spring of 1980, second stories with side-gabled roofs had been added to the original main block, and the east projection the porch's original flat roof

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Flagstaff Chamber of Commerce

Continuation Sheet No.3

had been replaced with a conical roof, the height and angle of the parapet wall between the main block and the eastern projection was increased; and the western projection was constructed.

ML-5 has not been moved and retains integrity of location. The building is along Historic Route 66 within the City of Flagstaff, and the surrounding area includes the BNSF Railroad to the south, downtown Flagstaff to the north, a vacant lot to the west, and a parking lot and the historic Santa Fe Passenger Depot to the east. The building's setting has not changed substantially since its construction and it retains integrity of setting. Although the 1980 additions and renovations to ML-5 occurred during the historic period defined of this study (prior to 1981), those additions and renovations included adding a second story and effectively removed most of the building's original, character-defining, Modern/Contemporary-style features. Other modifications to the building include the installation of the storefront system on the façade of the main block between 2016 and 2018, which originally featured ribbon of multi-light windows, and the replacement of the front entrance with new doors and the replacement of the windows throughout. Both the 1980 renovation and the later alterations to the building have substantially compromised the building's historic integrity of design, materials, workmanship, feeling, and association.

Although Site ML-5 is associated with the commercial development of Flagstaff in the 1950s, the Flagstaff Chamber of Commerce, and its growth in the post-World War II era, renovations to the building have resulted in a loss of integrity and it no longer conveys association with these themes and does not qualify for eligibility under NRHP Criterion A. Historic research did not identify any associations with significant persons in history, and the building does not qualify for eligibility under NRHP Criterion B. The renovations to ML-5 in 1980 masked its original, one-story, Modern/Contemporary design and the building does not possess sufficient integrity or architectural significance to qualify for eligibility under NRHP Criterion C. Furthermore, the building is not likely to yield information important to history or prehistory, and does not qualify for NRHP eligibility under Criterion D. Therefore, Site ML-5 is recommended not eligible for listing in the NRHP.

REFERENCES

Arizona Daily Sun. 1952a. \$5,000 Still Needed. 25 April, p. 1.

_____. 1952b. Chamber Gains Community Support. 8 May, p. 2.

_____. 1952c. Chamber of Commerce Home Started. 23 June, p. 1.

_____. 1952d. Chamber Building Rises. 30 July, p. 2.

_____. 1952e. C of C Home Half Finished. 8 August, p. 11.

_____. 1952f. At Cornerstone Rites. 11 September, p. 6.

_____. 1952g. Chamber of Commerce Now in New Home. 29 September, p. 3.

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Flagstaff Chamber of Commerce Continuation Sheet No.4

_____. 1952h. Public Invited to Open House At New C-C Home Saturday. 3 October, pp. 1, 5, and 8.

_____. 1952i. New C-C Quarters Officially Opened. 6 October, p. 1 and 5.

_____. 1979a. Chamber Remodeling. 11 September, p. 1.

_____. 1979b. Travelodge East to Donate To Chamber Building Fund. 18 October, p. 4.

_____. 1980. The Chamber's New Look. 30 March, p. 15.

Flagstaff Directory Company. 1959. *Flagstaff City Directory Including Oak Creek Canyon, Sedona, Bellemont, Parks, Grand Canyon, and Williams*. Flagstaff, Arizona.

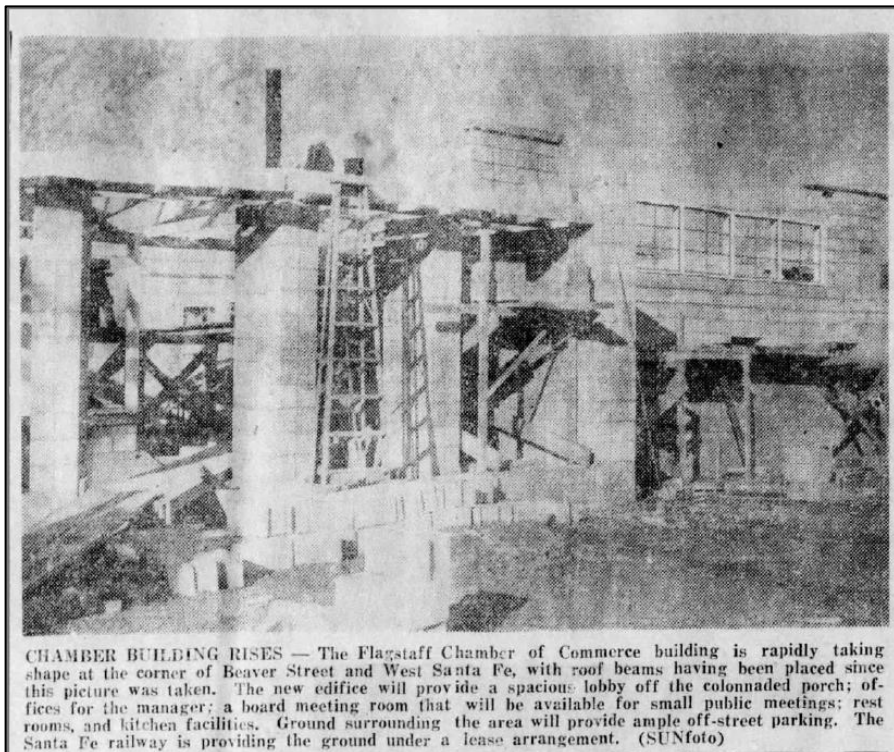
STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

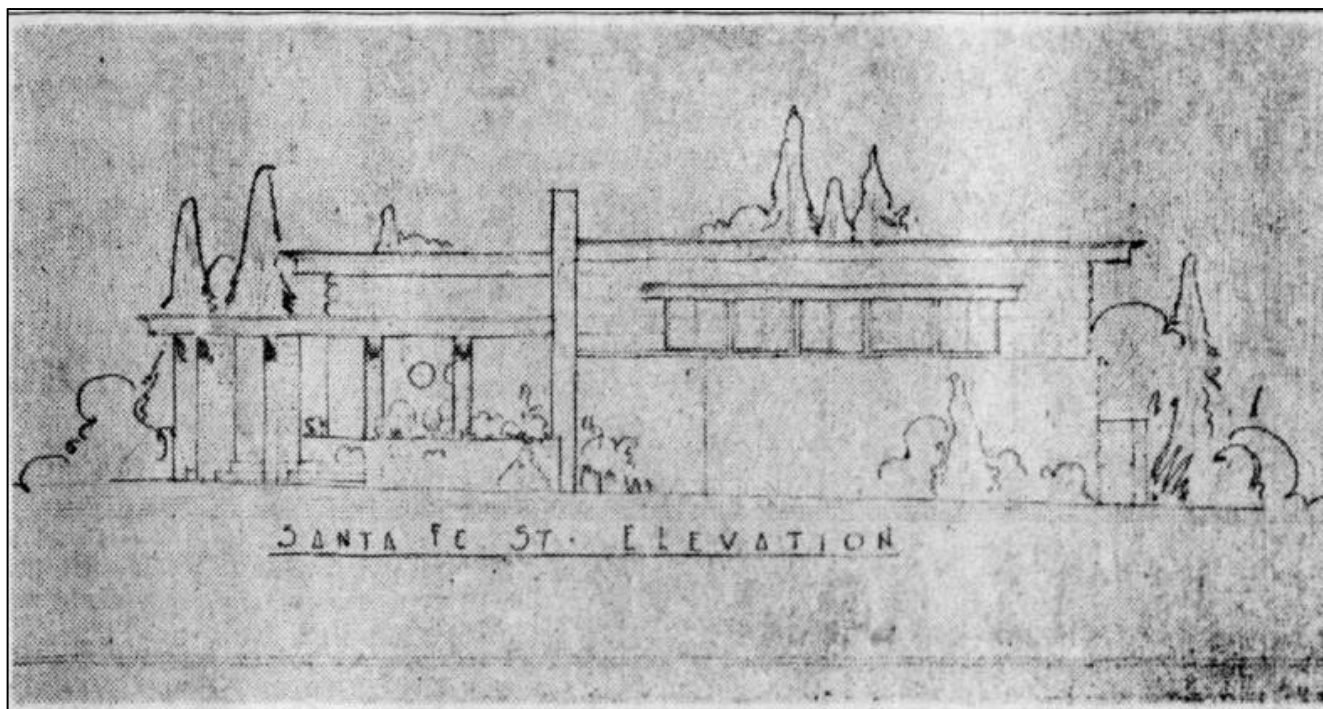
name of property Flagstaff Chamber of Commerce

Continuation Sheet No.5

PHOTOGRAPHS



ML-5 under construction in July 1952 (*Arizona Daily Sun* 1952d).



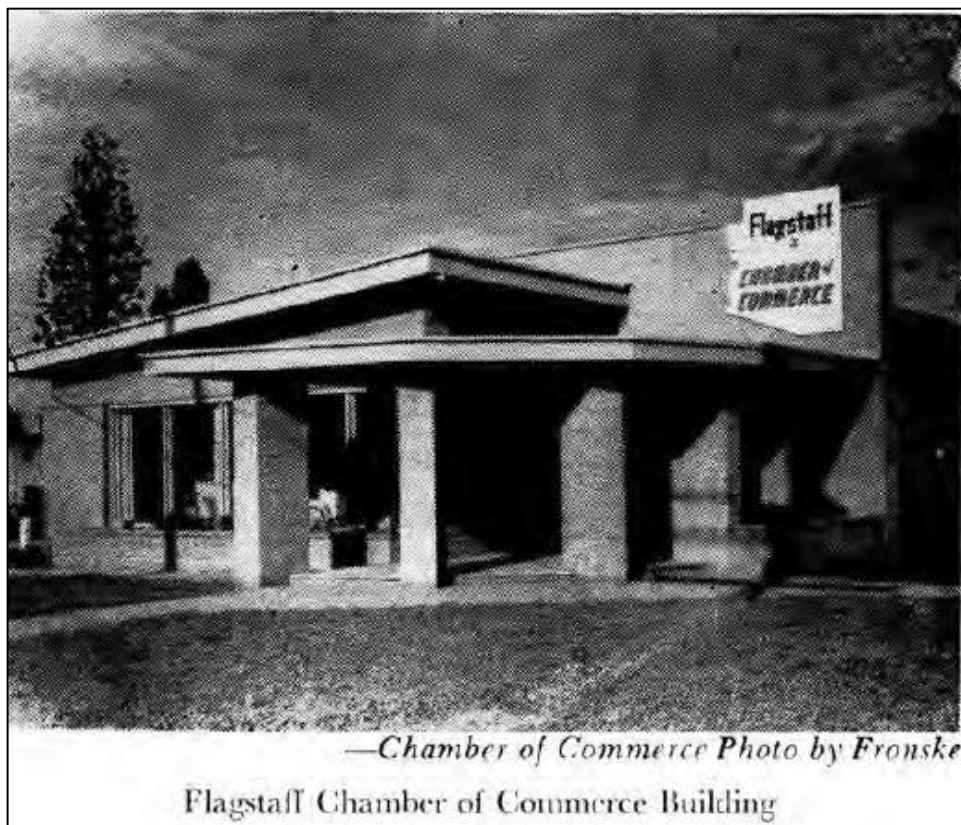
Architect's rendering of the façade of ML-5 (*Arizona Daily Sun* 1952a).

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Flagstaff Chamber of Commerce

Continuation Sheet No.6



View of the east elevation of ML-5 in the 1950s (Flagstaff Directory Company 1959).



View of the east elevation of ML-5 after the renovations (Arizona Daily Sun 1980).

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Flagstaff Chamber of Commerce

Continuation Sheet No.7



View of the east elevation of ML-5, looking southwest.



Detail of the porch and the primary entrance, looking west-northwest.

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

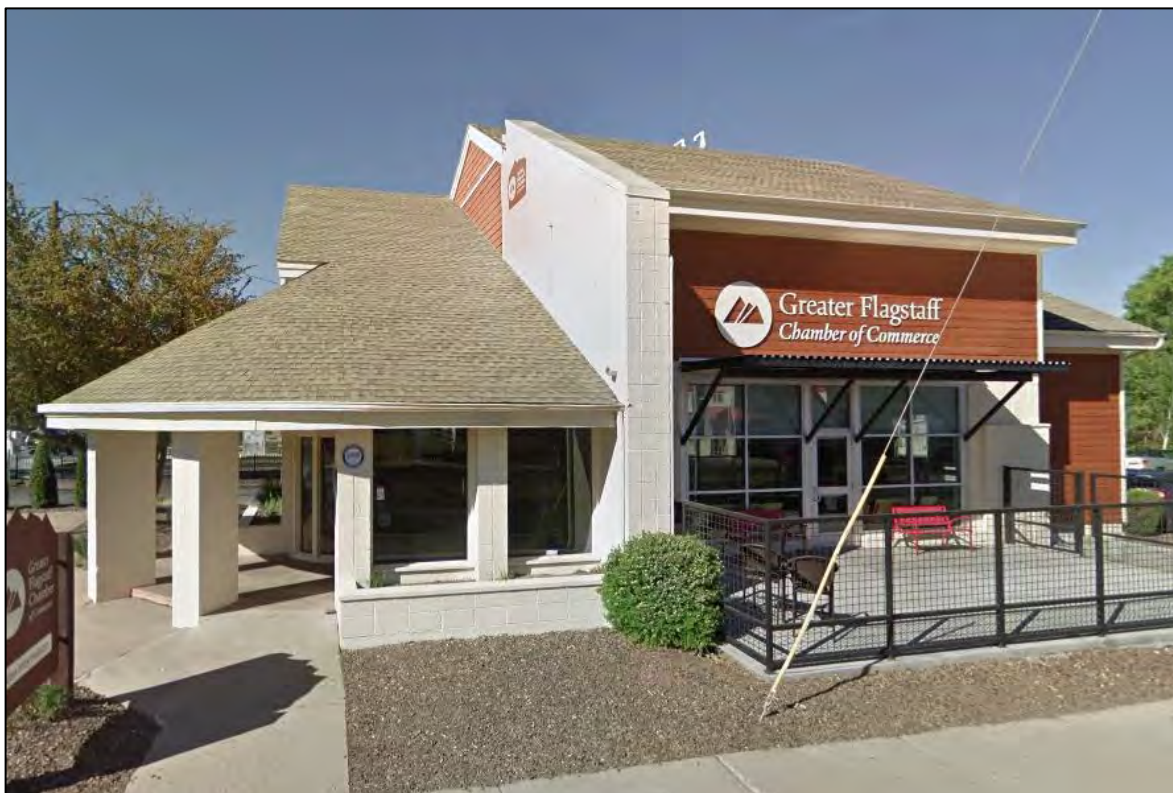
CONTINUATION SHEET

name of property Flagstaff Chamber of Commerce

Continuation Sheet No.8



Façade view of ML-5 in 2016, looking south (Google Image Capture, 10/2016).



Façade view of ML-5 in 2018, looking south (Google Image Capture, 6/2018)

**STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM**

CONTINUATION SHEET

name of property Flagstaff Chamber of Commerce

Continuation Sheet No.9



Oblique view of the façade and west elevation of ML-5, looking east.



Oblique view of the rear (south) and east elevations of ML-5, looking northwest.

STATE OF ARIZONA
HISTORIC PROPERTY INVENTORY FORM

CONTINUATION SHEET

name of property Flagstaff Chamber of Commerce

Continuation Sheet No.10



View of the rear (south) elevation of ML-5, looking north-northeast.



Aerial photograph of Site ML-5 (Google Earth, Imagery Date 5/23/2021)

Appendix C: Photographs



Photo 1. View of the Arizona Mineral Belt Railroad Trestle, looking southeast.



Photo 2. View from the west end of the Arizona Mineral Belt Railroad, looking southwest.



Photo 3. View of the Arizona Machinery Co. Building, the Hutchison & Sauer Building, the Hutchison Building, Marcos Café, looking southwest.



Photo 4. View of the Hutchison & Sauer Building, the Hutchison Building, and Marcos Cafe, looking southeast.



Photo 5. View toward the Project area from the Hutchison & Sauer and Hutchison buildings and Marcos Cafe, looking northwest.



Photo 6. View of the Coca-Cola Bottling Works, looking south-southwest.



Photo 7. View of the T.E. Pulliam House, looking southeast.



Photo 8. View of the Flagstaff Steam Laundry, looking southwest.



Photo 9. View toward the eastern section of the Project area from the vicinity of the Coca-Cola Bottling Works, the T.E. Pulliam House, and Flagstaff Steam Laundry, looking northeast.



Photo 10. View toward the central section of the Project area from the vicinity of the Coca-Cola Bottling Works, the T.E. Pulliam House, and Flagstaff Steam Laundry, looking north.



Photo 11. View toward the western section of the Project area from the vicinity of the Coca-Cola Bottling Works, the T.E. Pulliam House, and Flagstaff Steam Laundry, looking northwest.



Photo 12. View of the 5-story apartment building (The Jack) on the south side of W. Phoenix Avenue, looking southwest. The existing DCC is visible in the foreground.



Photo 13. View of the Arizona Central Commercial Company Warehouse, looking southeast.



Photo 14. View of the Du Beau Motel, looking west-southwest.

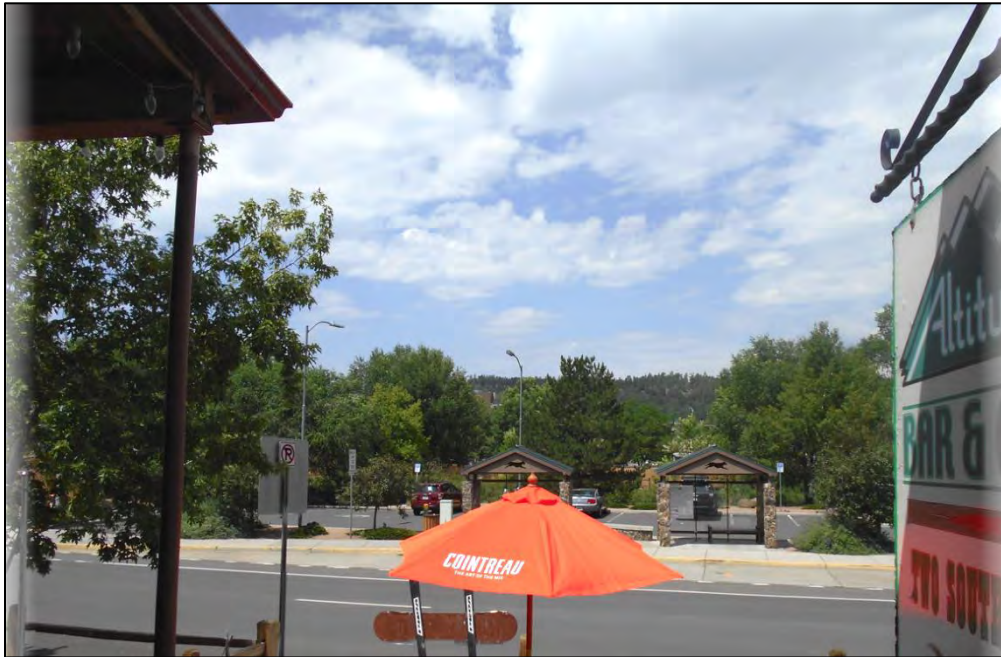


Photo 15. View toward the Project area from the facade of the Arizona Central Commercial Co. Warehouse, looking west.



Photo 16. View toward the Project area from the northeast corner of the intersection of S. Beaver St. and W. Phoenix Ave., looking west.



Photo 17. View toward the Project area from the southeast corner of the intersection of S. Beaver Street and W. Phoenix Ave., looking northwest.



Photo 18. View of the ATSF and the existing DCC, looking southwest.



Photo 19. View of an existing landscaped area and the ATSF north of the existing DCC, looking west.



Photo 20. View of the intersection of Route 66/US 89A/US 89 and W. Phoenix Avenue, looking east toward the Project area.

Memorandum

To	City of Flagstaff Development Services Department	Page	1 of 7
CC			
Subject	Northern Arizona Intergovernmental Public Transportation Authority (NAIPTA) Downtown Connection Center (DCC) Preliminary Drainage Design & Impact Memo		
From	Geoffrey Stone, PE		
Date	November 9, 2021		

Project Overview:

This project consists of the Phase I site improvements in support of the new 3.97 Acre Downtown Connection Center development located at 116 West Phoenix Avenue in Flagstaff, AZ including a new building, hardscape, and street improvements. New surface retention, underground retention, and storm drain infrastructure will be installed as part of the project. Additional information is shown for the Phase II improvements (bus plaza & civic space), and a conceptual layout of a future surface parking lot (to be installed by the City of Flagstaff at a later date) as they will partially drain towards the Phase I. Both of these improvement phases are conceptual as of this memo and are subject to change. Floodplain mapping revisions through FEMA and the City may be required during the final design phase to remove the building from the floodplain.

Design Criteria:

This project is subject to the requirements of the following manuals and guidelines:

1. City of Flagstaff (COF) Stormwater Management Design Manual
2. Low Impact Development Guidance Manual for Site Design and Implementation
3. MAG Uniform Standard Specifications & Details
4. COF Supplements to MAG Uniform Standard Specifications & Details
5. COF Standard Drawings
6. COF Adopted International Building Codes
7. COF Zoning Code
8. Engineer's Soils Report

Existing Site Conditions:

The existing project area contains a building, bus stops, parking areas, sidewalks, and an existing retention basin. The site generally slopes from north to south. There is a high point between the two

parking areas that split the area draining to Phoenix Avenue. The western side of the existing building drains toward Milton Road. The eastern portion of the project area drains to the nearby Rio De Flag channel.

On-site drainage infrastructure is limited. There is an existing retention basin that serves the eastern parking lot. Two existing sidewalk scuppers allow drainage to both enter and exit the site from Phoenix Avenue through the City sidewalk.

The site falls entirely within a Zone 'AE' floodplain per FEMA Flood Insurance Rate Map (FIRM) 04005C6809G effective 9/3/2010. A portion of this map is attached to this memo. Zone AE is defined as areas with a 1% annual chance of flooding. Zone AE is defined by FEMA and the FIRM Panel as a Special Flood Hazard Area subject to inundation by the 1% annual chance flood (100-year flood).

Proposed Site Conditions:

The proposed project will be constructed in two phases; Phase I will consist of a new building and hardscape and Phase II will build a new bus plaza and civic space (although this Site Plan package only includes Phase I, discussion of Phase II is included herein to make assumptions for the ultimate build out of the site). A subsequent project by the City of Flagstaff will build the northern portion of the property out as a surface parking lot. The proposed drainage areas will match the existing ones where possible with two notable exceptions. The high point in the center of the site has been shifted further west as the street grade will dictate the bus plaza grades in order to maintain ADA compliance within the pedestrian areas. Secondly, at the time of this submittal, it is anticipated that the Arizona Department of Transportation will be conducting a future widening project for Milton. The proposed site layout has shifted the building away from the existing property line to account for this. This has increased the drainage area on the west side of the building that drains toward Milton but detention facilities are proposed to mitigate this effect. The final project designer will work with the City to make sure concerns regarding this are addressed.

Drainage infrastructure will include a surface retention basin and an underground storage system with a storm drain system. Preliminary sizing calculations for the storage systems are included within this memo. Refer to the attached preliminary Grading & Drainage for an overview of all proposed improvements.

The proposed DCC site will mostly remain within the Zone AE floodplain. The building area will be elevated from the existing condition in order to limit floodproofing to the first 3 feet of building height. Currently, the Army Corps of Engineers is working on a large-scale storm drain system to reduce or eliminate portions of this floodplain as part of the Rio De Flag Flood Control Project. In order to avoid reliance on this project moving forward in a timely manner, the DCC project will be built based upon the effective floodplain conditions.

Rational Method Calculations:

Runoff Coefficients:

The runoff coefficients for the existing and proposed condition have been calculated per the COF Stormwater Management Design Manual:

Runoff Coefficient (C) Calculations					
Land Use	Runoff Coefficient	Existing Condition		Proposed Condition	
		Area (AC)	Weighted Coefficient	Area (AC)	Weighted Coefficient
Streets	0.95	0.00	0.00	0.05	0.01
Asphaltic Concrete	0.95	1.59	0.38	0.67	0.16
Concrete	0.95	0.09	0.02	1.01	0.24
Drives and Sidewalks	0.95	0.20	0.05	1.19	0.28
Roofs	0.95	0.51	0.12	0.24	0.06
Clay Soils (Woods; <2%)	0.10	1.57	0.04	0.80	0.02
Totals:		3.97	0.61	3.97	0.78

Proposed conditions assume the ultimate buildout of the site which includes Phase I & II of the DCC Project as well as a future parking lot project that is planned to be installed on-site north of the DCC development area.

Drainage Area Calculations:

Drainage delineations have been developed for the on-site areas based upon proposed conditions. This includes areas on the property that are outside of the limits of construction. Refer to the existing and proposed drainage area maps for the location of each area.

Peak Flow Calculations:

Peak flows have been calculated for the 100-year storm event per Equation 3.1 of Section 3.1.1 of the COF Stormwater Management Design Manual:

$$Q = C_f C I A$$

Where: Q = maximum rate of runoff, cfs

C_f = antecedent precipitation factor

C = runoff coefficient

I = rainfall intensity, in/hr

A = drainage area tributary to the design location, acres

PEAK FLOW CALCULATIONS						
Drainage Area (#)	Runoff Coefficient, C (Unitless)	Precipitation Factor, Cf* (Unitless)	Time of Concentration, Tc (Min)	Rainfall Intensity, I (In/Hr)	Area, A (Ac)	Peak Flow, Q (Cfs)
EX-1	0.61	1.25	5.0	8.52	0.10	0.6
EX-2	0.61	1.25	5.0	8.52	2.43	15.8
EX-3	0.61	1.25	5.0	8.52	1.10	7.1
EX-4	0.61	1.25	5.0	8.52	0.35	2.3
PR-1	0.78	1.25	5.0	8.52	0.13	1.1
PR-2	0.78	1.25	5.0	8.52	2.13	17.7
PR-3	0.78	1.25	5.0	8.52	1.28	10.6
PR-4	0.78	1.25	5.0	8.52	0.42	3.5

*C x Cf must be less than or equal to 1.00.

Detention & Retention Basin Design:

Per City guidance for the project, the greater of the detention capacity required to mitigate the peak flow to existing levels and the Runoff Capture Volume per the City's Low Impact Development (LID) requirements must be installed as part of the project. This have been calculated below and the required volume was determined on a per drainage area basis.

Detention Calculations:

Since the project area is mostly impervious and under 5 acres, the detention volumes have been determined for the 100-year storm event using the Modified Rational Graphical Hydrograph Method per Equation 8.2 of Section 8.3.1.2 of the COF Stormwater Management Design Manual:

$$V = 60 [C_f C_i A t - R (t_d + t_c) / 2]$$

Where: V = the required volume of the pond, cubic feet
 C = the post-development runoff coefficient
 Cf = the antecedent precipitation factor
 i = the rainfall intensity for t, in/hr
 R = the allowable release rate, cfs
 td = the storm duration to maximize the volume, min
 tc = the post-development time of concentration, min
 A = drainage area, acres

DETENTION VOLUME CALCULATIONS (MODIFIED RATIONAL METHOD)								
Drainage Area (#)	Runoff Coefficient, C (Unitless)	Precipitation Factor, Cf* (Unitless)	Rainfall Intensity, I (In/Hr)	Area, A (Ac)	Allowable Release Rate, R (Cfs)	Storm Duration, Td (Min)	Post-Dev. Time of Concentration, Tc (Min)	Storage Volume, V (Cft)
PR-1	0.78	1.25	8.52	0.13	0.6	5.0	5.0	146
PR-2	0.78	1.25	8.52	2.13	15.8	5.0	5.0	549
PR-3*	0.78	1.25	8.52	1.28	7.1	5.0	5.0	1039
PR-4*	0.78	1.25	8.52	0.42	2.3	5.0	5.0	364

*Provided for informational purposes only. These areas will be part of Phase II which will have a separate site plan submittal. Design of drainage facilities for these areas is not included as part of this memo and will be developed as part of Phase II.

Runoff Capture Volume (ROCV) Retention Calculations:

The required total ROCV for each drainage area consists of a combination of the existing retention/detention basin capacity plus 1" of rainfall depth across any increase in impervious area. For this project, the increased impervious area was calculated across the entire site and weighted for each drainage area as follows:

$$V_p = [I_p - I_e] \times [A / A_T] \times [1/12] + V_e$$

Where: V_p = the required volume of the proposed pond, cubic feet

I_e = the pre-development impervious area for the project area, square feet = 104,185 SF

I_p = the post-development impervious area for the project area, square feet = 137,835 SF

A = the contributing on-site drainage area, square feet

A_T = the total on-site drainage area, square feet = 172,731 SF

V_e = the existing retention/detention volume for the drainage area, cubic feet

EXISTING BASIN CAPACITY				
Drainage Area (#)	Elevation (Ft)	Area (Sf)	Volume (Cf)	Combined Volume (Cf)
PR-3*	6897.40	4767	0	0
	6896.60	3029	3118	3118
	6895.60	1118	2073	5191
	6895.20	85	241	5432

ROCV VOLUME				
Drainage Area (#)	Area, A (Sf)	First Flush Volume (Cf)	Existing Basin Capacity, V_e (Cf)	Required ROCV Volume, V_p (Cf)
PR-1	5853	95	0	95
PR-2	92886	1508	0	1508
PR-3*	55764	905	5432	6337
PR-4*	18228	296	0	310

REQUIRED BASIN VOLUME SUMMARY		
Drainage Area (#)	Required Volume** (Cf)	Basin Type
PR-1	146	Detention
PR-2	1508	Retention
PR-3*	6337	Retention
PR-4*	364	Detention

**Greater of the ROCV and Detention Volumes

PROPOSED BASIN CAPACITY				
Drainage Area (#)	Elevation (Ft)	Area (Sf)	Volume (Cf)	Combined Volume (Cf)
PR-1	6895.00	1503	0	0
	6894.00	904	1203	1203
PR-2	REFER TO STORMTECH DATA SHEET			1857

*Provided for informational purposes only. These areas will be part of Phase II which will have a separate site plan submittal. Design of drainage facilities for these areas is not included as part of this memo and will be developed as part of Phase II.

Refer to the attached proposed drainage area map for the location of each basin. Retention facilities will be required to drain within 36 hours which can be accomplished via bleed-off pipe or percolation which will be designed during the construction document phase.

Off-Site Flows:

The project area is subject to off-site flows associated with the Rio De Flag (RDF) Floodplain entering the site from east of the project area. This flow general travels north to south and exits along the southern property boundary. The floodway associated with the RDF channel will be impacted by the future Phase 2 civic space development. It will be the responsibility of the final designer to confirm the floodway limits, model existing and proposed conditions, and document a no-rise condition as required by the COF.

Finished Floor Elevation:

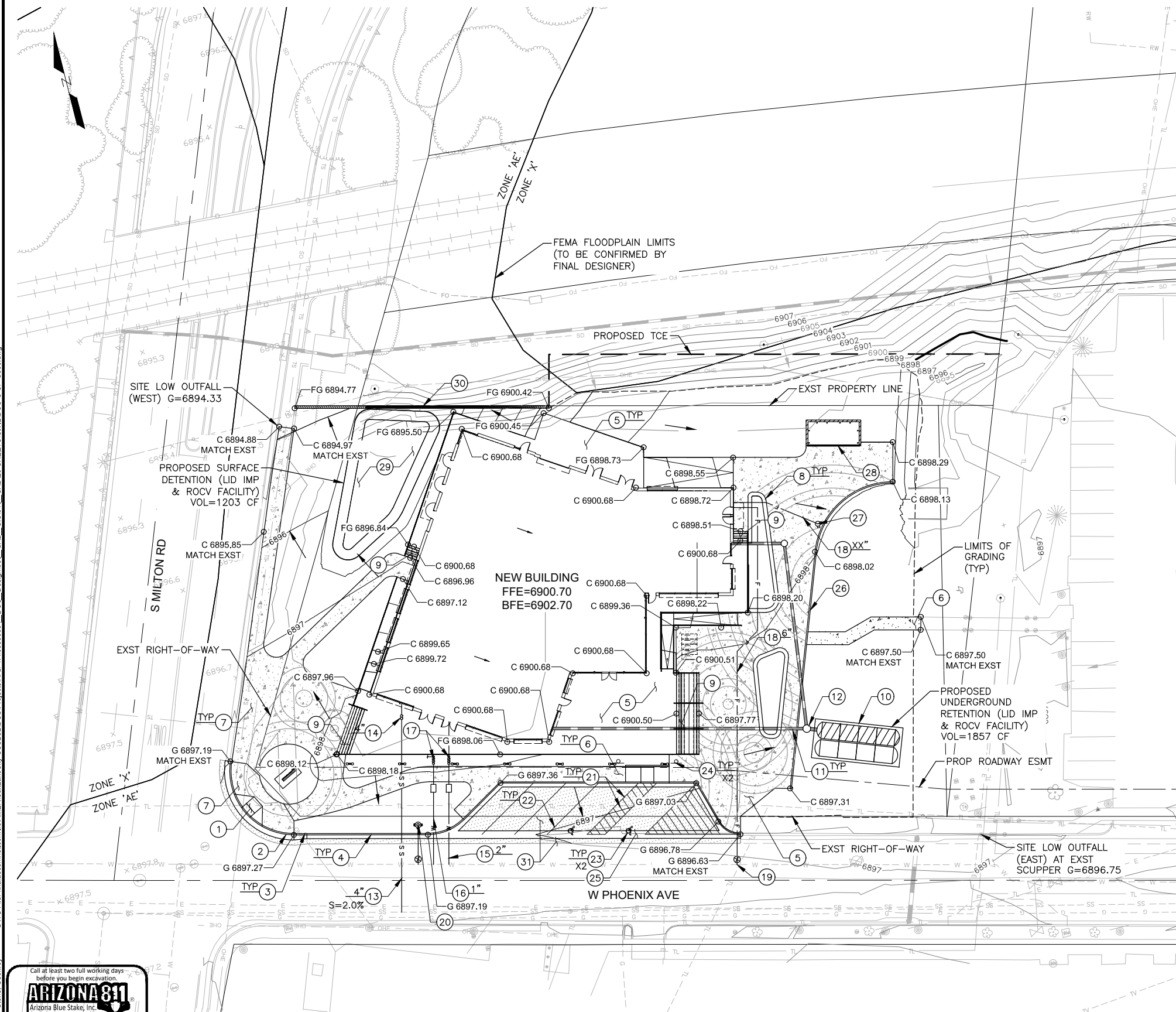
From the available information from FEMA, it was determined that the high Base Flood Elevation (BFE) at the new building location is 6902.70. Since this is substantially above existing grade, the building will have a finished floor elevation set at 6900.70 and an additional 3-feet of passive floodproofing that will provide protection up to an elevation of 6903.70, 12" above the BFE. It will be the responsibility of the final design team to incorporate floodproofing into the building design at during the construction document phase and to prepare floodplain mapping analysis and revision applications if required.

Drainage Impact Analysis:

Per the calculations contained with this memo, stormwater infrastructure will be installed to reduce the proposed runoff leaving the site to the existing peak flow levels or below. Impacts to the adjacent City stormwater infrastructure are not anticipated to increase. Floodplain modeling will be required by the final designer during the construction document phase to confirm the FEMA floodplain limits, and verify that both Phase I & II of the DCC project will result in a no-rise condition per previous request of the City.

Conclusions:

For Phase I, the DCC project will decrease stormwater flow exiting the site to pre-development levels by installing 3,060 CF (vs. 1,654 CF required) of new detention and retention facilities on-site. The proposed building will be floodproofed to an elevation of 6903.70 to protect it from the existing Zone AE floodplain that currently impacts the site.



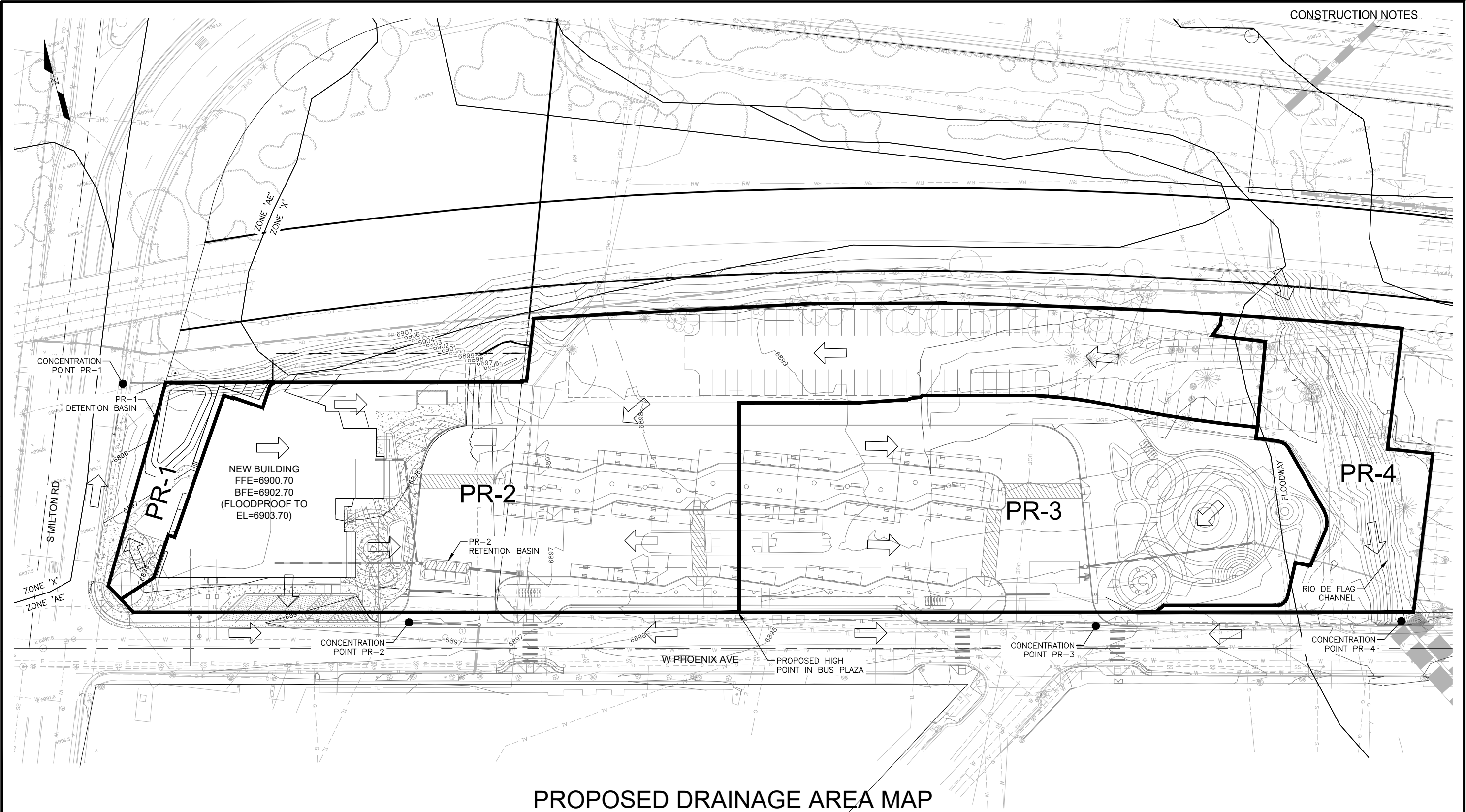
1" = 20' @ 24X36
CADD Filename: DCC-C-SP-C500.dwg
Submittal Date: 09/28/21
Contract No.:
Sheet No.: C501

DWG File: \\na.aecomnet.com\Users\AMER\Sailor\City-LSS\CD\Projects\NAIPTA\60959394_DCC_Design\900_CAD_GIS\910_CAD\20-SHEETS\CIVIL\Drainage\Exhibits\DCC-DRNG-AREA-EXBT-2.dwg

Stone, Geoffrey

09/04

9-Nov-2021



CONSTRUCTION NOTES

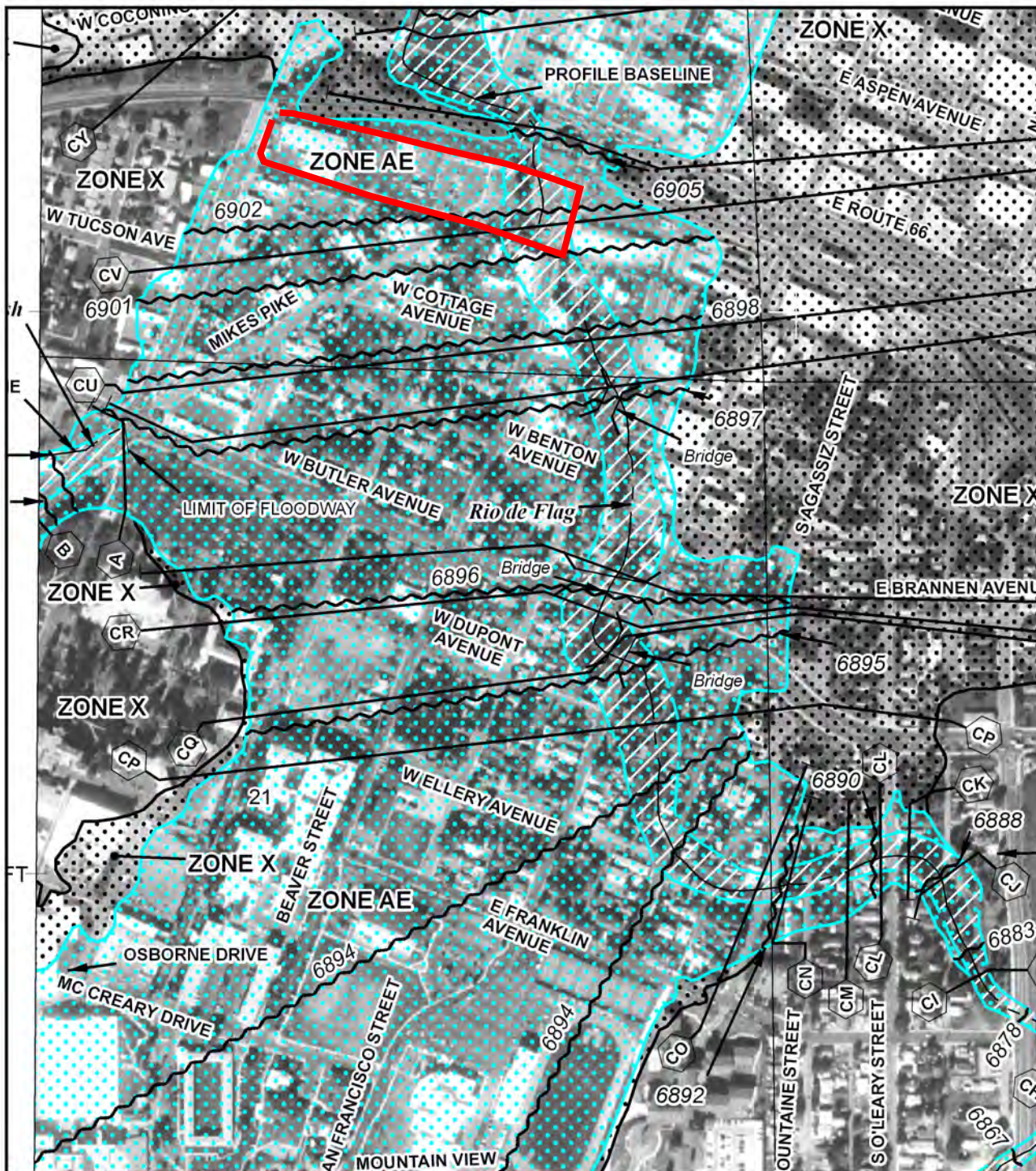
PROPOSED DRAINAGE AREA MAP



- NOTES:
1. ASSUMED DRAINAGE PATTERNS ARE SHOWN IN AREAS OUTSIDE OF PHASE I FOR PURPOSES OF ESTIMATING FULL BUILD-OUT CONDITION DRAINAGE FACILITIES.
 2. INFORMATION SHOWN IN NORTH PARKING LOT IS CONCEPTUAL IN NATURE AND SUBJECT TO CHANGE.



			Submittal:	Seal:	Consultant:	Client:	Designed By:	NAIPTA MOUNTAIN LINE DOWNTOWN CONNECTION CENTER (DCC) - PHASE ONE 216 W. PHOENIX AVENUE, FLAGSTAFF, AZ 86001 ASSESSOR'S PARCEL NUMBER (APN) 1004405A, 10043003B, 10043001D PROPERTY OWNER: BRYCE DOTY, CITY OF FLAGSTAFF, (928) 637-8384 DEVELOPER: KATE MORLEY, MOUNTAIN LINE, (928) 679-8903 3773 N. KASPER DRIVE, FLAGSTAFF, AZ 86004 PREPARER: JENNIFER LOVE, AECOM, (602) 648-2423		1" = 30' @ 24X36 CADD Filename: DCC-DRNG-AREA-EXBT-2.dwg Submittal Date: 09/28/21 Contract No.: Sheet No.: DRNG EXBT 2
			30% PLANS	PRELIMINARY NOT FOR CONSTRUCTION	AECOM 7720 North 16th Street Suite 100 Phoenix, AZ 85020 Phone: (602) 371-1100	 	G. STONE			
							Drawn By:			
							G. CHU			
							Checked By:			
							M. ACEVES			
							Approved By:			
							M. ACEVES			
0	09/28/21	SITE PLAN SUBMITTAL								
REV	DATE	DESCRIPTION								



300 METERS

09G

FIRM

FLOOD INSURANCE RATE MAP

COCONINO COUNTY,
ARIZONA
AND INCORPORATED AREAS

PANEL 6809 OF 8475

(SEE MAP INDEX FOR FIRM PANEL LAYOUT)

CONTAINS:

COMMUNITY	NUMBER	PANEL	SUFFIX
COCONINO COUNTY	040019	6809	G
FLAGSTAFF, CITY OF	040020	6809	G

Notice to User: The Map Number shown below should be used when placing map orders; the Community Number shown above should be used on insurance applications for the subject community.



MAP NUMBER
04005C6809G

EFFECTIVE DATE
SEPTEMBER 3, 2010

Federal Emergency Management Agency

This is an official FIRMette showing a portion of the above-referenced flood map created from the MSC FIRMette Web tool. This map does not reflect changes or amendments which may have been made subsequent to the date on the title block. For additional information about how to make sure the map is current, please see the Flood Hazard Mapping Updates Overview Fact Sheet available on the FEMA Flood Map Service Center home page at <https://msc.fema.gov>.

User Inputs

Chamber Model:	MC-3500
Outlet Control Structure:	No
Project Name:	
Engineer:	N/A
Project Location:	
Measurement Type:	Imperial
Required Storage Volume:	1578 cubic ft.
Stone Porosity:	40%
Stone Foundation Depth:	9 in.
Stone Above Chambers:	12 in.
Average Cover Over Chambers:	18 in.
Design Constraint Dimensions:	(20 ft. x 40 ft.)

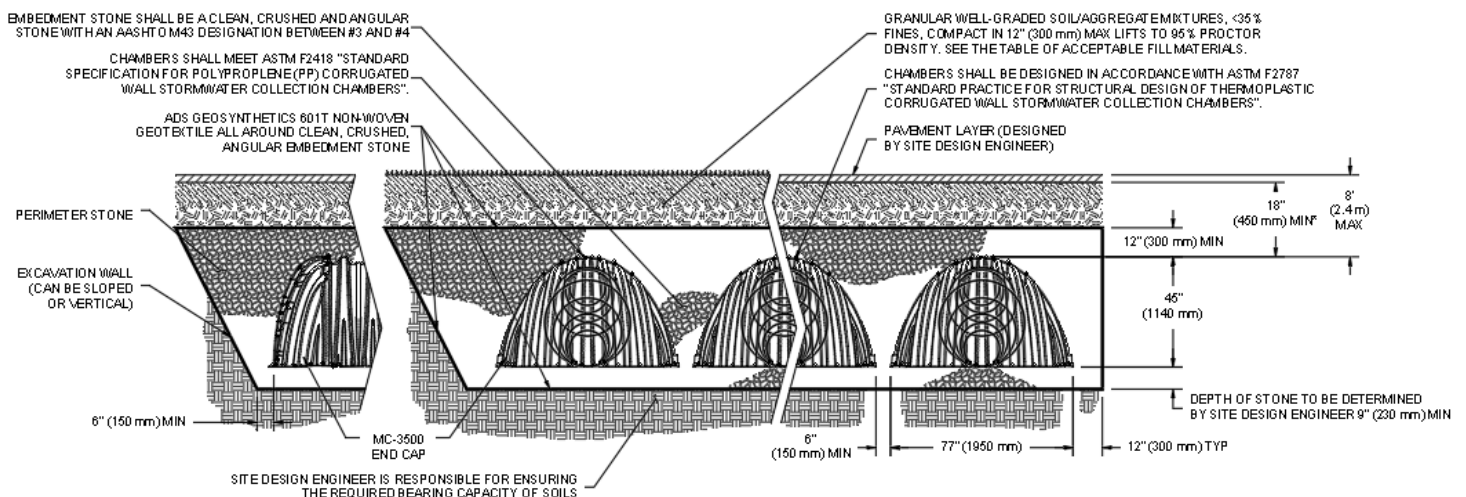
Results

System Volume and Bed Size

Installed Storage Volume:	1856.88 cubic ft.
Storage Volume Per Chamber:	109.90 cubic ft.
Number Of Chambers Required:	8
Number Of End Caps Required:	4
Chamber Rows:	2
Maximum Length:	38.35 ft.
Maximum Width:	15.33 ft.
Approx. Bed Size Required:	588.00 square ft.

System Components

Amount Of Stone Required:	85.01 cubic yards
Volume Of Excavation (Not Including Fill):	119.78 cubic yards
Total Non-woven Geotextile Required:	235.53 square yards
Woven Geotextile Required (excluding Isolator Row):	16.73 square yards
Woven Geotextile Required (Isolator Row):	37.82 square yards
Total Woven Geotextile Required:	54.55 square yards



*MINIMUM COVER TO BOTTOM OF FLEXIBLE PAVEMENT. FOR UNPAVED INSTALLATIONS WHERE RUTTING FROM VEHICLES MAY OCCUR, INCREASE COVER TO 24"

Memorandum

To	City of Flagstaff Development Services Department	Page	1 of 1
CC			
Subject	NAIPTA Downtown Connection Center (DCC) Site Plan Application Water & Sewer Impact Analysis Exception Memo		
From	Geoffrey Stone, PE		
Date	November 9, 2021		

Water & Sewer Impact Analysis Exception:

Per Section 13-05-002-001 & 13-05-002-0002 of the City of Flagstaff Engineering Design Standards and Specifications for New Infrastructure, a Water and Sewer Impact Analysis is required for Site Plan submittals for “any development which will generate a peak hour demand (or discharge) greater than the equivalent flow of ten (10) single-family dwelling units”. The proposed DCC project contains a single commercial building. Please see the attached calculations and summary below:

NAIPTA DCC Demand:

The basis of this calculation is the 2018 International Plumbing Code, Table E103.3, and the proposed plumbing fixture counts. Water use is based on qty. (129) full-time-equivalent employees and qty. (50) visitors per day. Calculations assume water and sanitary demand are equivalent with no loss between supply and discharge rates. Refer to attached calculation for proposed water consumption analysis.

- Maximum Daily Consumption: 347 gallons per day (gpd)
- **Peak Water and Sanitary Flow Rate: 347 gpd / 24 hr per day = 14.5 gph**

Ten (10) Single-Family Dwelling Demand:

The basis of this calculation is the procedure outlined in Title 13, Section 13-09-002-0002 and Table 9-1 (Wastewater Design Flows), Section 13-05-002-0001 (Water Impact Analysis). Calculations assume water and sanitary demand are equivalent with no loss between supply and discharge rates. The residential, single-family, townhomes shall be based on 3.5 persons per dwelling unit and 75 gallons per capita, per day.

- Maximum Daily Consumption: 75 gallons per capita per day (gpcd)
- Design Population: 10 units x 3.5 people per unit = 35 people
- Maximum Daily Consumption: 35 people x 75 gpcd = 2,625 gallons per day (gpd)
- **Peak Water and Sanitary Flow Rate: 2,625 gpd / 24 hr per day = 109 gph**

Based on this comparison of proposed facility to residential dwelling baseline, the proposed NAIPTA DCC building will consume less than the ten (10) single family homes equivalent; therefore, a Water and Sewer Impact Analysis is not required for this project.



Water Consumption Calcs

(Based on LEED v4) *

Sheet 1 of 1

Project: **NAIPTA**

Building Occupants

Total Building Area: 19,347 SF
Occupant Density: 150 SF/Person Based on Architectural Analysis (if available)
Area-Based Occ: 129 People
Occupant Override: 129 People If Qty of Occupants is known

Gender Ratio: 50% Male
50% Female

Percent of males expected to use Urinals 100%

**Enter 100% if all male restrooms have urinals, 0% if the project contains no urinals, etc.)

Annual Days of Operation: 365 Days

**Enter the number of days the project is accessible to employees or FTE.

Default Uses (Employees): 8 hours per day, 52 weeks per year, 5 days per week

Non-Default (Visitors): 12 HOURS PER DAY, 52 WEEKS PER YEAR, 7 DAYS PER WEEK

Daily Visitors (Transient FTEs) 50 People

For Project with dual-flush Water Closets:

Low Flush (gpf): 1.1 GPF
Full Flush (gpf): 1.6 GPF
Weighted average flush rate (gpf): 1.35 GPF

Table 1: Flush Fixtures

FIXT. ID	FIXTURE FAMILY	FIXTURE TYPE	Baseline	Design	Percent	Uses Per Day					Total Daily Uses		Total Daily Water Use	
			Flush Rate	Flush Rate	of	Employees (FTE)	Visitors	Retail Customers	Students (K-12)	Other (specify)	Employee Use	Visitor Use (optional)	Baseline (gallons)	Design (gallons)
			[GPF]	[GPF]	Occupants									
WC-1	Water Closet (Male)	Low-Flow Water Closet	1.6	1.1	50%	2	0.5				129	25.0	40	27.5
WC-1	Water Closet (Female)	Low-Flow Water Closet	1.6	1.1	50%	3	1				193.5	50.0	80	55.0
UR-1	Urinal	Low-Flow Urinal	1.0	0.125	50%	1	0.5				65	25.0	25	3.1

Baseline Case Daily Flush Volume (gallons per day): 145 GPD
Baseline Case Annual Flush Volume (gallons per year): 52,925 GPY
Design Case Daily Flush Volume (gallons per day): 86 GPD
Design Case Annual Flush Volume (gallons per year): 31,253 GPY

Table 2: Flow Fixtures

FIXT. ID	FIXTURE FAMILY	Duration		Baseline	Design	Percent	Uses Per Day					Total Daily Uses		Total Daily Water Use	
		Default (Seconds)	Non-Default (Seconds)	Flow Rate	Flow Rate	of	Employees (FTE)	Visitors	Retail Customers	Students (K-12)	Other (specify)	Employee Use	Visitor Use (optional)	Baseline (gallons)	Design (gallons)
				[GPM]	[GPM]	Occupants									
LAV-1	Public Lavatory (restroom) Faucet	30		0.5	0.5	100%	3	50				387	50.0	109.3	109.3
-	Private (residential) Lavatory Faucet	60		0.8	0.8		0	0				0	0.0	0.0	0.0
KS-1	Kitchen Faucet	15		1.8	1.0	100%	1	0				129	0.0	56.4	32.3
-	Residential Kitchen Faucet	60		0.5	0.5		0	0				0	0.0	0.0	0.0
-	Showerhead	300		2.2	1.5		0	0				0	0.0	0.0	0.0
-	Residential Showerhead	480		2.2	1.5		0	0				0	0.0	0.0	0.0
-	Utility Sink		120	3.0	1.5		1	0				0	0.0	0.0	0.0
MSB-1	Mop Sink Basin		120	3.0	1.5	2%	1	0				2.58	0.0	15.5	7.7
WH-1	Wall Hydrant		900	5.0	5.0	1%	1	0				1.29	0.0	96.8	96.8
DW-1	Dishwasher		1800	0.2500	0.1250	2%	1	0				2.58	0.0	19.4	9.7
DF-1	Drinking Fountain	15		0.1250	0.125	100%	1	50				129	50.0	5.6	5.6

Baseline Case Daily Flow Volume (gallons per day): 303 GPD
Baseline Case Annual Flow Volume (gallons per year): 110,544 GPY
Design Case Daily Flow Volume (gallons per year): 261 GPD
Design Case Annual Flow Volume (gallons per year): 95,359 GPY

Table 3: Summary

Baseline Case:			Design Case:			Reduction / Water Savings		
Daily Flush Volume	145 GPD		Daily Flush Volume	86 GPD		Daily Flush Volume	59 GPD	41%
Daily Flow Volume	303 GPD		Daily Flow Volume	261 GPD		Daily Flow Volume	42 GPD	14%
Daily Consumption	448 GPD		Daily Consumption	347 GPD		Daily Consumption Savings	101 GPD	23%
Annual Flush Volume	52,925 GPY		Annual Flush Volume	31,253 GPY		Annual Flush Volume	21,672 GPY	41%
Annual Flow Volume	110,544 GPY		Annual Flow Volume	95,359 GPY		Annual Flow Volume	15,185 GPY	14%
Annual Consumption	163,469 GPY		Annual Consumption	126,613 GPY		Annual Consumption Savings	36,857 GPY	23%

Notes and Documentation:

General Notes

- A. This tab is used to estimate the water consumption by the building.
All fixtures, equipment and appliances connected to the domestic water system should be indicated here.

Pioneer Title Agency Inc.

REPORT OF TITLE

OUR NO. 90301610

Dated: October 22, 2021 at 7:30 a.m.

Fee: \$400.00

Report is issued for the sole use and benefit of:

**Tierra Right of Way Services
Corey Long
11022 N. 28th Drive, Suite 155
Phoenix, AZ 85029**

Pioneer Title Agency Inc. hereby reports that an examination of the title to the land described in Schedule A discloses that title is vested as shown in Schedule A, subject to the liens, encumbrances, and defects as shown in Schedule B.

This report is **FOR INFORMATIONAL PURPOSES ONLY**. It is neither a guarantee of title, a commitment to insure title nor a policy of title insurance.

SCHEDULE A

1. Title to the estate or interest covered by this report at the date hereof is vested in:

City of Flagstaff, an Arizona municipal corporation

2. The estate or interest in the land hereinafter described in this report is a fee.

3. The land referred to in this report is situated in the County of **Coconino**, State of Arizona, and is described as follows:

See Exhibit A attached hereto and made a part hereof.

All recording references are to records in the office of the County Recorder of the county in which the property is situated.

Pioneer Title Agency Inc.



By _____
Authorized Officer or Agent

Pioneer Title Agency Inc.

REPORT OF TITLE (Continued)

SCHEDULE B

At the date hereof exceptions to title are:

1. TAXES AND ASSESSMENTS collectible by the County Treasurer, a lien payable but not yet due for the following year:

Second half of 2021
2. MATTERS SHOWN ON Drawing showing location of railroad right-of-way and pipeline;

Filed : November 11, 1905
Drawing No. : [115-6000](#)
3. EASEMENT and rights incident thereto, as set forth in instrument:

Recorded in Docket : 168
Page : [300](#)
Purpose : Pipeline
4. MATTERS SHOWN ON SURVEY:

Recorded in File : 1 of Maps
Page : [01](#)
5. MATTERS SHOWN ON SURVEY:

Recorded in File : 1 of Maps
Page : [33](#)
6. MATTERS SHOWN ON SURVEY:

Recorded in Case : 3 of Maps
Page : [119](#)
7. AGREEMENT according to the terms and conditions contained therein:

Purpose : Construction of improvements
Dated : August 16, 1989
Recorded : October 10, 1989
Docket : 1304
Page : [01](#)
8. LICENSE and rights incident thereto, as set forth in instrument:

Recorded in Docket : 1472
Page : [42](#)
Purpose : Pipeline

Pioneer Title Agency Inc.

REPORT OF TITLE (Continued)

9. MATTERS SHOWN ON SURVEY:

Recorded in Book : 17 of Surveys
Page : [42](#)

10. The effect of Resolution by the City of Flagstaff, recorded in

Docket : 2179
Page : [463](#)
AND
Document No. : [3078057](#)
Concerning : Flood control and redevelopment

11. MATTERS SHOWN ON SURVEY:

Recorded in Document No. : [3373794](#)

12. The effect of Floodplain Permit by the City of Flagstaff, recorded in

Docket : 1721
Page : [578](#)

13. THE RIGHT OF ENTRY to prospect for, mine and remove the minerals in said land, as implied by the reservation of same in instrument:

Recorded in Document No. : [3009853](#)

14. EASEMENT and rights incident thereto, as set forth in instrument:

Recorded in Document No. : [3009853](#)
Purpose : Fiber optic lines, communication lines and facilities

15. EASEMENT and rights incident thereto, as set forth in instrument:

Recorded in Document No. : [3087029](#)
AND
Recorded in Document No. : [3089421](#)
Purpose : Ingress and egress for construction of flood control facilities

16. The effect of Ordinance by the City of Flagstaff, recorded in

Document No. : [3441093](#)
Concerning : Acquisition of real estate

17. The effect of Conditional Use Permit by the City of Flagstaff, recorded in

Document No. : 3472111
AND
Re-recorded in Document No. : [3491233](#)

Pioneer Title Agency Inc.

REPORT OF TITLE (Continued)

18. THE EFFECT OF ANY ACT OF CONGRESS of the United States of America, any Statute of the State of Arizona, or of any decision of any court of competent jurisdiction, limiting or purporting to limit the right of a railroad corporation to dispose of any of its operative property.

TAX NOTE:

Year	2021
Parcel No.	100-43-003B
Total Tax	EXEMPT

End of Exceptions

Pioneer Title Agency Inc.

REPORT OF TITLE (Continued)

Exhibit A

All that portion of The Burlington Northern and Santa Fe Railway Company's (formerly Atchison, Topeka and Santa Fe Railway Company) Station Ground Property in Flagstaff, Arizona situated in the Southeast quarter of Section 16, Township 21 North, Range 7 East of the Gila and Salt River Base and Meridian, Coconino County, Arizona described as follows:

BEGINNING at the intersection of the Northerly line of Phoenix Avenue, according to the recorded plat of Flagstaff, recorded in Book 1 of Maps, page 16, with a line drawn parallel with and distant 700.00 feet Westerly of, as measured at right angles from the Southerly extension of the Westerly line of Beaver Street, as set forth on the recorded plat;

THENCE Westerly along said Northerly line of Phoenix Avenue for a distance of 249.5 feet;

THENCE Northwesterly in a straight line a distance of 25.33 feet to the East line of the Underpass of US Highway 66 (Sitgreaves Street);

THENCE Northeasterly along said East line of US Highway 66 for a distance of 147.4 feet;

THENCE Easterly parallel with said North line of Phoenix Avenue for a distance of 226.43 feet;

THENCE Southerly in a straight line to the POINT OF BEGINNING;

EXCEPTING all coal, oil, gas, casing head gas and all ores and minerals of every kind and nature, and all water, underlying the surface of said premises, as reserved in instrument recorded in Instrument No. 3009853, records of Coconino County, Arizona.

ALTA/NSPS LAND TITLE SURVEY

LOCATED IN THE SOUTHEAST QUARTER OF SECTION 16, TOWNSHIP 21 NORTH, RANGE 7 EAST,
 OF THE GILA AND SALT RIVER MERIDIAN, COCONINO COUNTY, ARIZONA.

LEGAL DESCRIPTION

PER PIONEER TITLE AGENCY INC. REPORT OF TITLE NO. 90301610 DATED AUG. 27, 2021 AT 7:30AM

All that portion of The Burlington Northern and Santa Fe Railway Company's(formerly Atchison, Topeka and Santa Fe Railway Company) Station Ground Property in Flagstaff, Arizona situated in the Southeast quarter of Section 16, Township 21 North, Range 7 East of the Gila and Salt River Base and Meridian, Coconino County, Arizona described as follows:

BEGINNING at the intersection of the Northerly line of Phoenix Avenue, according to the recorded plat of Flagstaff, recorded in Book 1 of Maps, page 16, with a line drawn parallel with and distant 700.00 feet Westerly of, as measured at right angles from the Southerly extension of the Westerly line of Beaver Street, as set forth on the recorded plat;

THENCE Westerly along said Northerly line of Phoenix Avenue for a distance of 249.5 feet;

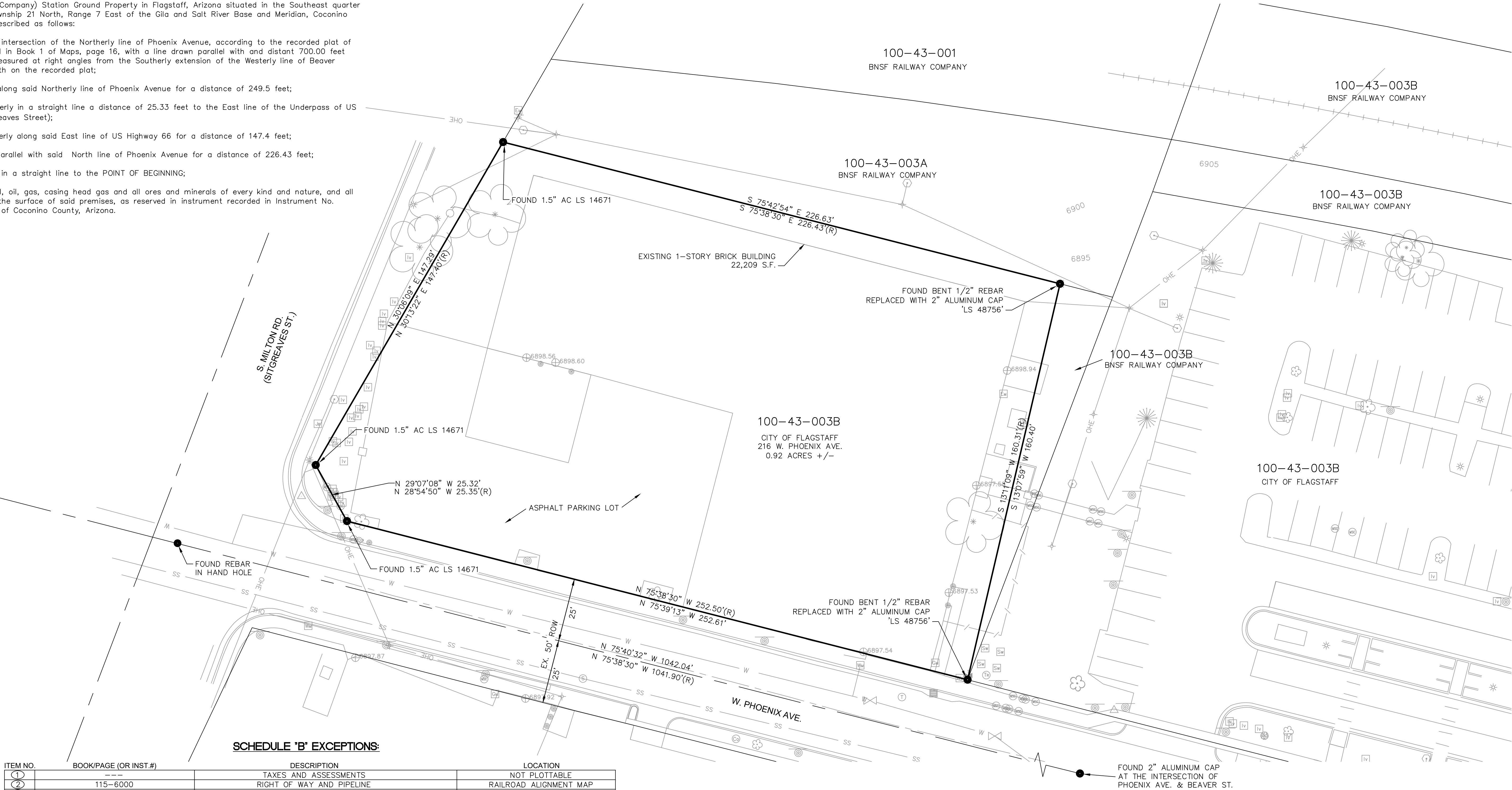
THENCE Northwestery in a straight line a distance of 25.33 feet to the East line of the Underpass of US Highway 66 (Sitgreaves Street);

THENCE Northeastery along said East line of US Highway 66 for a distance of 147.4 feet;

THENCE Easterly parallel with said North line of Phoenix Avenue for a distance of 226.43 feet;

THENCE Southerly in a straight line to the POINT OF BEGINNING;

EXCEPTING all coal, oil, gas, casing head gas and all ores and minerals of every kind and nature, and all water, underlying the surface of said premises, as reserved in instrument recorded in Instrument No. 3009853, records of Coconino County, Arizona.



SCHEDULE 'B' EXCEPTIONS:

ITEM NO.	BOOK/PAGE (OR INST.#)	DESCRIPTION	LOCATION
(1)	---	TAXES AND ASSESSMENTS	NOT PLOTTABLE
(2)	115-6000	RIGHT OF WAY AND PIPELINE	RAILROAD ALIGNMENT MAP
(3)	168/300	PIPELINE EASEMENT	DOES NOT AFFECT SUBJECT PARCEL
(4)	BK 1 OF MAPS, PG 1	SURVEY MAP	FLAGSTAFF TOWNSITE
(5)	BOOK 1 OF MAPS, PG 33	SURVEY MAP	MAP OF FLAGSTAFF
(6)	CASE 3, PAGE 119	SURVEY MAP	RAILROAD LAND GRANT MAP
(7)	1304/01	IMPROVEMENTS AGREEMENT	DOES NOT AFFECT SUBJECT PARCEL
(8)	1472/42	PIPELINE LICENSE	DOES NOT AFFECT SUBJECT PARCEL
(9)	BK 17 OF SURVEYS, PG 42	SURVEY MAP	(R) AS SHOWN
(10)	2179/463, 3078057	RESOLUTION FOR FLOOD CONTROL AND REDEVELOPMENT	NOT PLOTTABLE
(11)	3373794	SURVEY MAP	SURVEY CONTROL PLAN
(12)	1721/578	FLOODPLAIN PERMIT	NOT PLOTTABLE
(13)	3009853	RIGHT OF ENTRY	SUBJECT PARCEL
(14)	3009853	EASEMENT FOR COMMUNICATION LINES	SUBJECT PARCEL
(15)	3087029, 3089421	FLOOD CONTROL EASEMENT	SUBJECT PARCEL
(16)	3441093	ORDINANCE FOR ACQUISITION OF REAL ESTATE	SUBJECT PARCEL
(17)	3472111, 3491233	CONDITIONAL USE PERMIT	SUBJECT PARCEL
(18)	---	ACTS OF CONGRESS	---

TO: TIERRA RIGHT OF WAY SERVICES, CITY OF FLAGSTAFF, AND PIONEER TITLE AGENCY, INC.

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2021 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1, 8, 11, 16, 17, AND 19 OF TABLE A THEREOF.

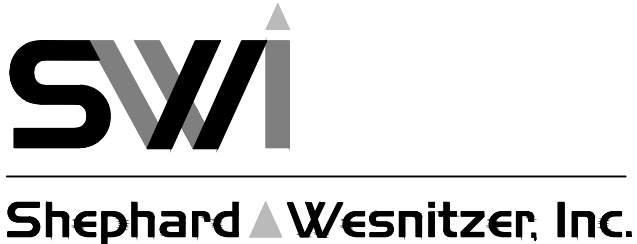
DATE OF MAP: SEPTEMBER 27, 2021

Aaron D. Borling

Aaron D. Borling, RLS 48756



REVISIONS			
NO.	DESCRIPTION	DATE	BY



110 W. Dale Avenue
 Flagstaff, AZ 86001
 928.773.0354
 928.774.8934 fax

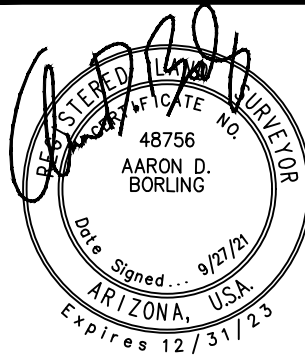
www.swiaz.com

JOB NO:	20305
DATE:	SEP. 21
SCALE:	1"=20'
DRAWN:	ADB
DESIGN:	
CHECKED:	ADB

APN: 100-43-003B

FLAGSTAFF,
 ARIZONA

ALTA/NSPS LAND TITLE SURVEY



DRAWING NO.

A1

SHT NO. OF
 1 1

To	City of Flagstaff Development Services Department	Page	1 of 3
CC			
Subject	NAIPTA Downtown Connection Center (DCC) Site Plan Application Preliminary Logistics Report		
From	Miguel Aceves, PE		
Date	November 9, 2021		

Project Overview:

This project consists of site improvements in support of the new Downtown Connection Center (DCC) development located at 116 West Phoenix Avenue in Flagstaff, AZ including a new building, bus plaza, civic center, and street improvements constructed in phases. Phase 1 consists of the new building, on street parking, and adjacent plaza, and will tie into the existing parking lot.

Existing Site Conditions:

The existing site for Phase 1 is approximately 0.5 acre and is located at 216 Phoenix Avenue south of the downtown area in the City of Flagstaff, Arizona. The existing site contains a building last renovated in 2011. The existing site for Phase 2 is located on the central and eastern portion of the site (parcels APN 100-43-001D and APN 100-44-005A) and it consists mainly of parking areas and bus bays.

Construction Logistics:

Phase 2 of the project is located on the same site as a U.S. Army Corps of Engineers (USACE) Rio de Flag flood control project. The Rio de Flag flood control project proposes to bury a concrete box to convey off-site flows. Due to the location and timing of the USACE Rio de Flag flood control project, construction of the DCC project will be split into phases to be completed on different timelines pending Rio de Flag flood control project construction.

Phase 1 – Building Demolition and Construction

Phase 1 consists of demolishing the existing building and constructing the proposed new DCC building, surrounding infrastructure, and public spaces adjacent to the building. This phase is located on the western portion of the site (parcel APN 100-43-003B).

Construction of this phase would begin in early 2022 and take approximately ten months (February through November 2022). The existing surface parking lots and transit operations on parcel APN

10044005A would remain. A temporary trailer will be installed to provide driver amenity space, and connect the trailer to utilities, during construction of this phase. The temporary trailer will be located on the northwest corner of the existing parking area, impacting six parking spaces as shown on Exhibit A.

No road closures are anticipated. Temporary lane restrictions along the north side of Phoenix Avenue, between Milton Road and Mikes Pike Street, are expected for construction of curbs and sidewalks. All materials and equipment are expected to stay within the property.

The anticipated equipment required for this phase includes: Large track excavator with grapple, multiple tandem axle dump trucks, D7 bull dozer, Large Front-End loader, cutting torches, and concrete delivery trucks.

Phase 2 – Bus Plaza and Civic Space

Phase 2 consists of demolition and construction of the bus plaza and civic space, and is located on the central and eastern portion of the site (parcels APN 100-43-001D and APN 100-44-005A).

Construction of this phase is estimated to begin in 2025, pending completion of the USACE Rio de Flag flood control project. Construction would take place over two construction seasons (May through October), for a total duration of 13 months. Transit operations would be relocated during construction.

It is anticipated construction will require temporary sidewalk and roadway closures along Phoenix Avenue. All materials and equipment are expected to stay within the property.

The anticipated equipment required for this phase includes: D5 bulldozer, medium size excavator, backhoe, tandem axle dump trucks, all terrain equipment handler, 60 ton crane, smooth drum vibratory compactor, mobile air compressor, mobile welding unit, concrete delivery trucks, large track excavator, multiple tandem dump trucks, D7 bull dozer, and large front end loader, concrete delivery trucks

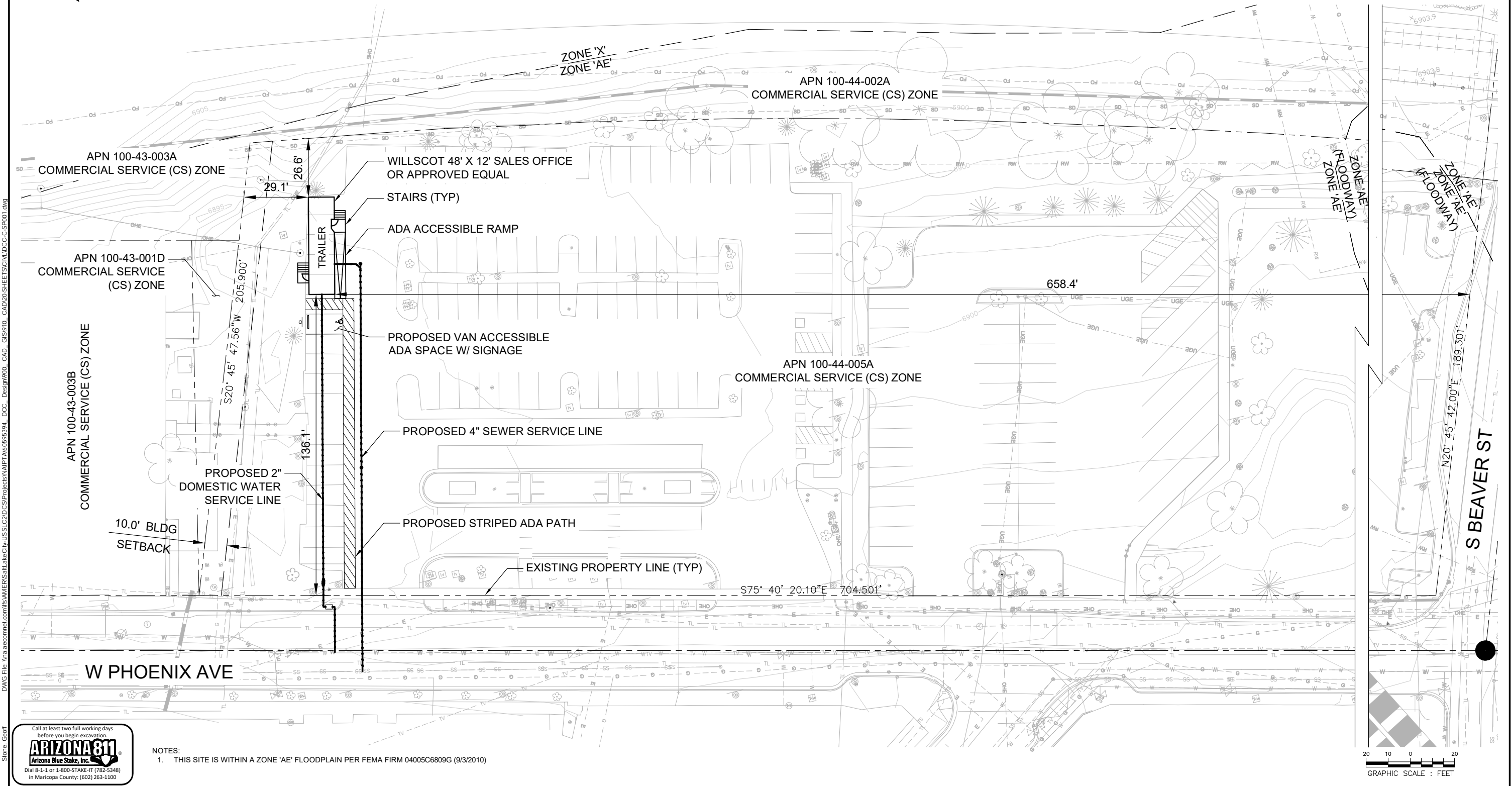
Conclusions:

The construction of the DCC project is expected to be phased between years 2022 and 2025. Minor restrictions to facilities within the right-of-way are expected, and all materials and equipment to be stored within the property.


Exhibit A

TEMPORARY CONSTRUCTION TRAILER SITE PLAN

116 W PHOENIX AVE, FLAGSTAFF, AZ 86001



NOTES:
1. THIS SITE IS WITHIN A ZONE 'AE' FLOODPLAIN PER FEMA FIRM 04005C6809G (9/3/2010)

15:55	-	-	-	Submittal:	Seal:	Consultant:	Client:	Designed By:	NAIPTA DOWNTOWN CONNECTION CENTER	1" = 20' @ 24X36
	-	-	-							CADD Filename:
	-	-	-							DCC-C-SP001.dwg
	-	-	-							Submittal Date:
	-	-	-							06/18/21
	-	-	-							Contract No.:
20-Jun-2021	REV	DATE	DESCRIPTION	SITE PLAN	PRELIMINARY NOT FOR CONSTRUCTION	756 East Winchester Street Suite 400 Salt Lake City, UT 84107 Phone: (801) 904-4000 Fax: (801) 904-4100	 	M. ACEVES	TEMPORARY DRIVER RELIEF FACILITY 116 W PHOENIX AVE, FLAGSTAFF, AZ 86001	TBD
			Sheet No.:							
			SP001							
				Submitted By:		Approved By:		M. ACEVES		

To: Michael Ashcraft, ADOT**Project name:** Mountain Line DCC
Design/NEPA**CC:** George Williams, ADOT
Kate Morley, Mountain Line
Anne Dunno, Mountain Line
Alex Ortiz, Nations Group
Todd Shafer, Nations Group
Jennifer Love, AECOM
Scott Shea, AECOM**Project ref:** 60595394**From:** Kordel Braley, PE, PTOE, Senior
Traffic Engineer**Date:** November 2, 2021

Memorandum: Signalizing Milton Road and Phoenix Avenue Intersection

This memorandum provides an overview of the benefits and justification for signalizing the intersection of Milton Road and Phoenix Avenue (the intersection) as part of the Downtown Connection Center (DCC) redevelopment. Additional information and data is available in the Traffic Impact Analysis.

Benefits/Justification to Signalizing Milton Road/Phoenix Avenue Intersection

1. The intersection was recommended for signalization by the Milton Road Corridor Master Plan.
2. Meets ADOT signal warrant analysis.
3. Reduces delay for westbound right (WBR) movements waiting behind vehicles making the westbound left (WBL) at the intersection.
4. Signal timing can be offset to avoid interrupting platooned vehicles along Milton Road, thus reducing the impact of intersection to Milton Road.
5. A draft traffic impact analysis (TIA) performed by AECOM shows no significant negative traffic operational affects are likely due to installation of a signal.
6. The signal will reduce delay and improve reliability for buses which have higher person carrying capacity than standard automobiles.
7. Allows bus routes to be modified for efficiency, which eliminates bus traffic making WBL at congested Butler Avenue and Milton Road intersection.
8. Reduces bus traffic and route retracing along Butler Avenue.
9. Improved safety for WBL vehicles turning onto Milton Road at the intersection.

Signalizing Phoenix Ave & Milton Road – October 29, 2021

10. Improved safety for southbound (SBL) vehicles turning onto Phoenix Avenue at the intersection.
11. Improved safety for pedestrians crossing Milton Road at the intersection.
12. Improves access and mobility for active transportation by connecting the Title VI neighborhood La Plaza Vieja to Southside/downtown area.
13. Improves system resiliency by creating a secondary egress for southbound buses. Mountain Line buses are only able to effectively enter their southbound routes at intersections with traffic control during peak hour operations.
14. Signalizing the intersection will also result in geometric changes to the east leg of the intersection. Northbound right buses take both lanes to make the turn, but the modifications will change the turning path and increase safety.
15. Operationally it creates a time savings for buses.
16. Increases on time performance, a key performance measure for the Federal Transit Administration.